

# PLAN OF PROPOSED IMPROVEMENTS FOR CTH Y RESURFACING STH 153 - CTH II MARATHON COUNTY, WISCONSIN



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### LEGEND & ABBREVIATIONS

LEGEND	
▲ - SURVEY CONTROL POINT	— — — — — BUILDING (OBJECT) LINE
● - FD. LP./REBAR	— — — — — ROAD/PARKING LOT EDGE
⊙ - FIRE HYDRANT	— x — x — x — FENCE
□ - UTILITY POLE	— T — T — UTILITY LINE
← - GUY WIRE	— SAN — UTILITY LINE
⊠ - SIGN/BILLBOARD	— — — — — TREE LINE
● MH - MANHOLE	— — — — — CONTOURS INTERMEDIATE
⊙ GV - VALVE (TYPE)	— — — — — CONTOURS INDEX
⊙ - LIGHT POLE	— — — — — PROPERTY BORDER
⊙ - BENCH MARK	— — — — — PROPERTY LINE
⊠ - INLET	— — — — — LOT LINE
⊙ - TREE	— — — — — RIGHT OF WAY LINE
⊙ - PINE TREE	⊙ # — — — — — PLAN DETAIL
⊙ # - SECTION DETAIL	

ABBREVIATIONS	
T - TELEPHONE	O.H.E. - OVERHEAD ELECTRIC
G - GAS	F.O. - FIBER OPTIC
E - ELECTRIC	S.S. - STORM SEWER
W.M. - WATER MAIN	SAN. - SANITARY SEWER
MH - MANHOLE	F.M. - FORCE MAIN

### PROJECT DIRECTORY

**OWNER**  
MARATHON COUNTY HIGHWAY DEPARTMENT  
1430 WEST STREET  
WAUSAU, WI 54401  
PH: (715) 261-1800  
ATTN: JIM GRIESBACH

**CIVIL ENGINEER**  
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310 E. JACKSON STREET  
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ATTN: SCOTT WHITSETT  
EMAIL: SCOTT.WHITSETT@JEWELLASSOC.COM

**DEPARTMENT OF NATURAL RESOURCES**  
WISCONSIN RAPIDS SERVICE CENTER  
473 GRIFFITH DRIVE  
WISCONSIN RAPIDS, WI 54494  
ATTN: MARC HERSHFELD  
PH: (715)-421-7867  
EMAIL: MARC.HERSHFELD@WISCONSIN.GOV

**UTILITIES**  
GAS & ELECTRIC  
WISCONSIN PUBLIC SERVICE  
700 N. ADAMS STREET  
GREEN BAY, WI 54301  
ATTN: MIKE BOSI - GAS  
PH: (715) 848-7471  
EMAIL: MABOSI@WISCONSINPUBLICSERVICE.COM  
ATTN: CLAY VIRCKS - ELECTRIC  
PH: (715) 848-7317  
EMAIL: CHVIRCKS@WISCONSINPUBLICSERVICE.COM

CABLE TV/FIBER OPTIC/TELEPHONE  
WITTENBERG TELEPHONE COMPANY  
104 W. WALKER STREET  
PO BOX 160  
WITTENBERG, WI 54499  
ATTN: DAVE SCHMIDT  
PH: (715) 881-0308

TELEPHONE  
FRONTIER COMMUNICATIONS  
1851 N 14TH AVENUE  
WAUSAU, WI 54401  
ATTN: WARREN INMAN  
PH: (715) 847-1504

### GENERAL NOTES

COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS).

THE EXACT LOCATION AND WIDTH OF PRIVATE, COMMERCIAL, AND FILED ENTRANCES TO BE DETERMINED BY THE ENGINEER IN THE FIELD. EXISTING ENTRANCES WILL BE RESTORED IN KIND AS DIRECTED BY THE ENGINEER IN THE FIELD.

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA WHICH ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND OR A DIRECT CALL TO THE UTILITIES WHICH HAVE FACILITIES IN THE AREA.

ACCESS TO ALL ENTRANCES SHALL BE MAINTAINED DURING CONSTRUCTION.

3.5 INCHES OF HMA PAVEMENT TYPE E-1 SHALL BE CONSTRUCTED WITH A LOWER LAYER OF 1.75 INCHES AND AN UPPER LAYER OF 1.75 INCHES. THE NOMINAL SIZE OF AGGREGATE USED SHALL BE 12.5 MM.

REMOVAL OF ASPHALTIC SURFACES WHERE AN ABUTTING ASPHALTIC SURFACE IS TO REMAIN IN PLACE SHALL REQUIRE A SAWCUT MEETING THE APPROVAL OF THE ENGINEER IN THE FIELD.

HMA PAVEMENT TYPE E-1 QUANTITIES WERE CALCULATED BASED ON DENSITY OF 115 LB/SY-IN.

TACK COAT HAS BEEN ESTIMATED AT AN APPLICATION RATE OF 0.025 GAL/SY.

PAVING LIMITS AT INTERSECTIONS AND ENTRANCES SHALL MATCH THE EXISTING RADII AND TAPER LENGTHS.

### DRAWING INDEX

CIVIL	TITLE SHEET
C1.0	TYPICAL SECTIONS
C2.0 - C2.3	CONSTRUCTION DETAILS
C2.4 - C2.10	TRAFFIC CONTROL PLAN
C2.11	SUPERELEVATION TABLES
C2.12 - C2.13	STATION/COORDINATE TABLES
C2.14	PLAN VIEWS
C2.15 - C2.20	MISCELLANEOUS QUANTITIES
C3.0	

### DESIGN DATA

DESIGN SPEED	45/50/55 MPH
AADT (2013)	1050
AADT (2033)	1600

### PROJECT SCOPE

PULVERIZE AND RELAY, HMA PAVEMENT TYPE E-1, AND BASE AGGREGATE SHOULDERS

CTH Y 5.633 MILES

### ORIGINAL PLANS PREPARED BY:



*Scott Whitsett* 4/23/13  
SIGNATURE DATE

DESIGN IS IN ACCORDANCE WITH "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS" 2004 (AASHTO) EXCEPT AS PROVIDED FOR IN THE SPECIAL PROVISIONS. ALL WORK SHALL BE ACCOMPLISHED ACCORDING TO THE STATE OF WISCONSIN "STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION" 2013 EDITION.

### MARATHON COUNTY

RECOMMENDED FOR APPROVAL

*James M. Sulback* 4/23/13  
HIGHWAY COMMISSIONER DATE

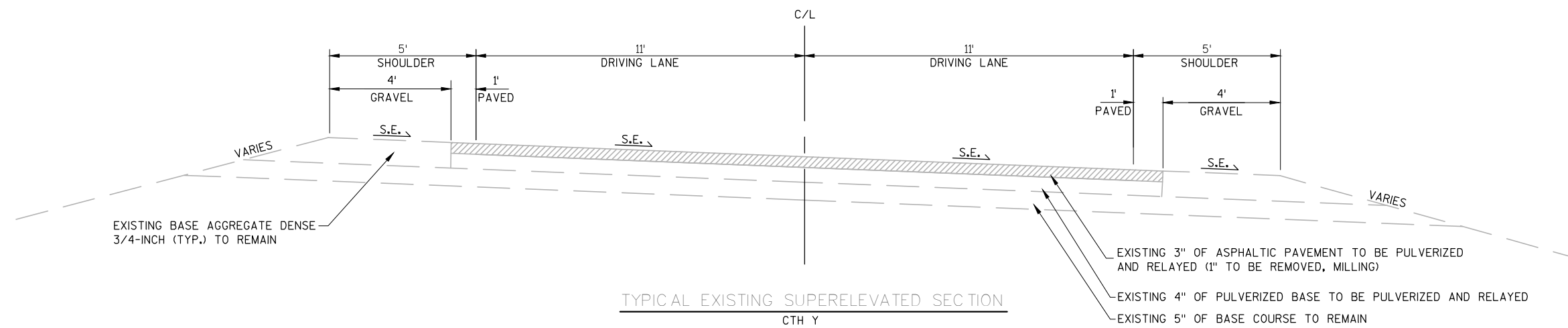
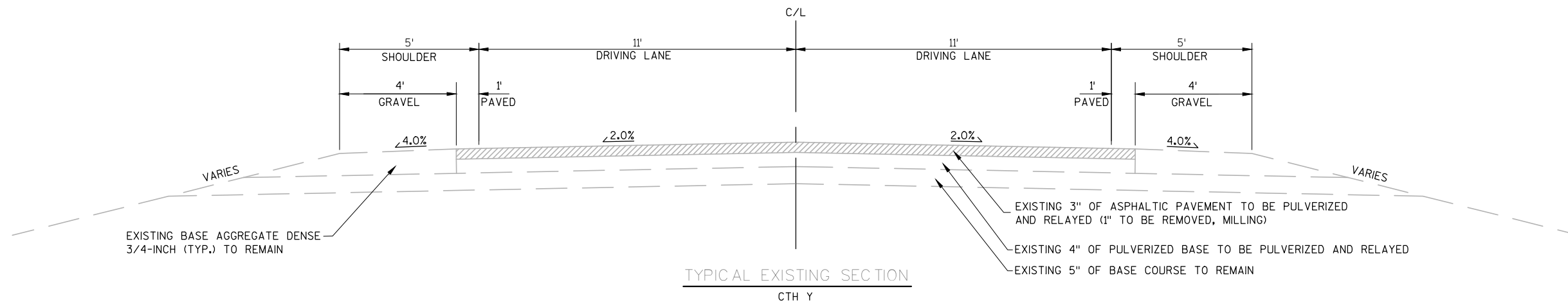
CTH Y  
STH 153 - CTH II  
MARATHON COUNTY, WISCONSIN

Date  
02-21-2013

Date Revision

Drawing Name  
TITLE SHEET

Sheet No.  
C1.0  
Jewell Project Number  
M97020



**CTH Y  
 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**

Date  
**4/17/2013**


Date Revision

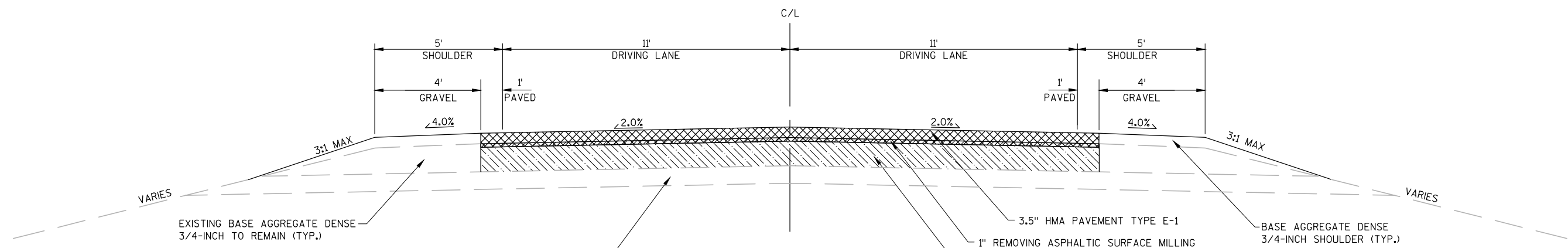
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**TYPICAL  
 SECTIONS**

Sheet No.  
**C2.0**

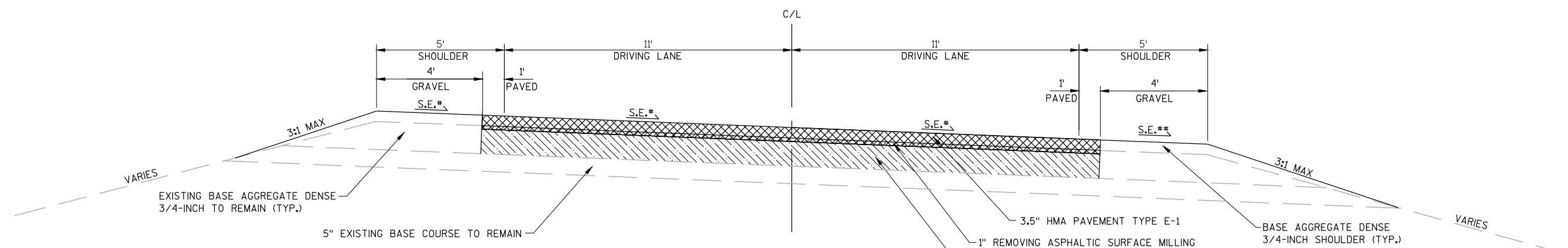
Jewell Project Number  
**M97020**

**CTH Y  
 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**



**TYPICAL FINISHED SECTION**

- STA. 10+50.00 - STA. 11+97.43
- STA. 19+90.90 - STA. 21+34.79
- STA. 30+82.82 - STA. 53+39.72
- STA. 62+56.12 - STA. 79+03.88
- STA. 87+71.18 - STA. 90+82.52
- STA. 98+42.67 - STA. 113+64.54
- STA. 118+65.46 - STA. 127+26.87
- STA. 137+09.55 - STA. 144+27.15
- STA. 154+65.51 - STA. 167+07.78
- STA. 177+90.00 - STA. 178+32.73
- STA. 185+41.84 - STA. 191+32.71
- STA. 245+47.14 - STA. 307+90.00



**TYPICAL FINISHED SECTION**

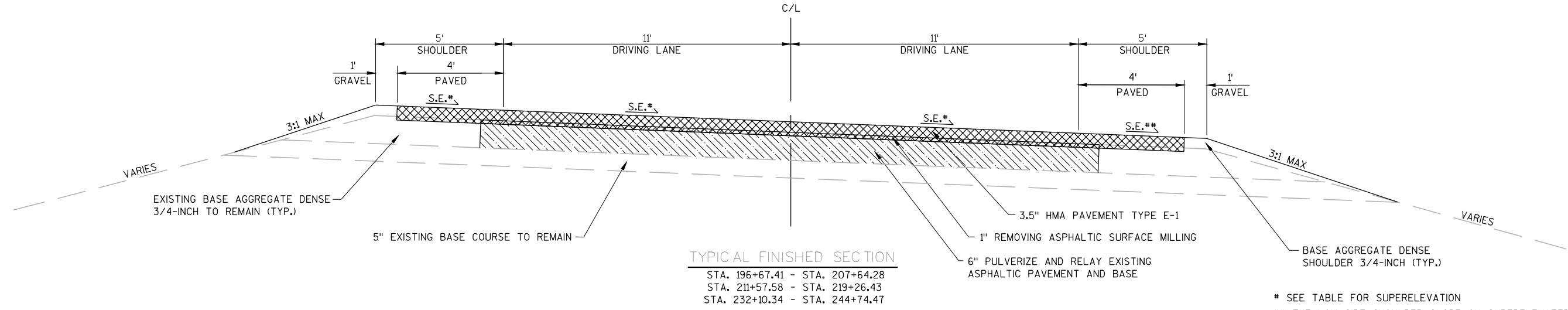
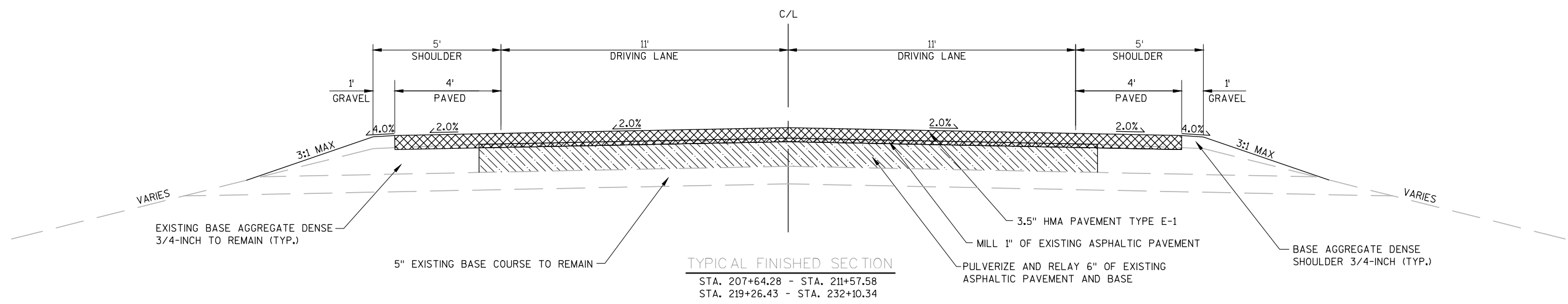
- STA. 11+97.43 - STA. 19+90.90
- STA. 21+34.79 - STA. 30+82.82
- STA. 53+39.72 - STA. 62+56.12
- STA. 79+03.88 - STA. 87+71.18
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- STA. 144+27.15 - STA. 154+65.51
- STA. 167+07.78 - STA. 177+90.00
- STA. 178+32.73 - STA. 185+41.84
- STA. 191+32.71 - STA. 196+67.41
- STA. 244+74.47 - STA. 245+47.14

\* SEE TABLE FOR SUPERELEVATION  
 \*\* THE LOW SIDE SHOULDER SLOPE ON SUPERELEVATED SECTIONS EQUALS THE SUPERELEVATION WHEN THE SUPERELEVATION IS GREATER THAN 0.04 FT./FT. IF THE SUPERELEVATION IS LESS THAN OR EQUALS 0.04 FT./FT., THEN THE LOW SIDE SHOULDER SLOPE IS 0.04 FT./FT. THE HIGH SIDE SHOULDER SLOPE ON THE SUPERELEVATED SECTION EQUALS THE SUPERELEVATION.

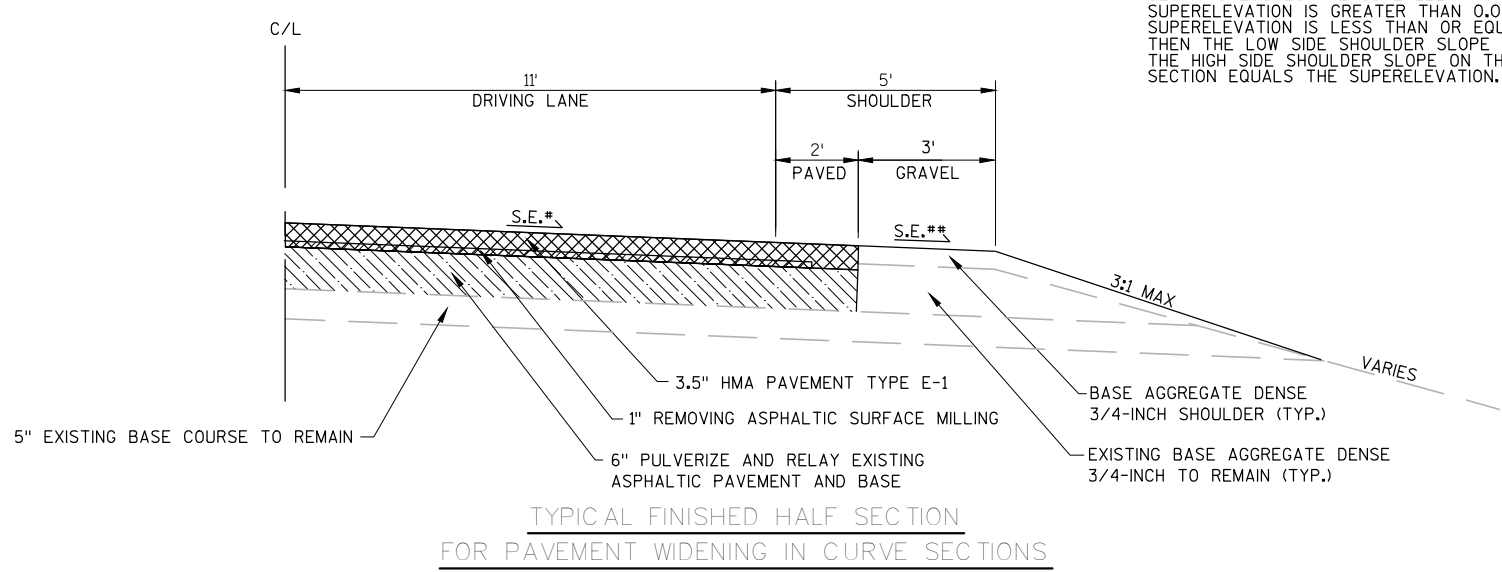
Date	Revision
4/17/2013	

Drawing Name  
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Sheet No.  
**C2.1**  
 Jewell Project Number  
**M97020**

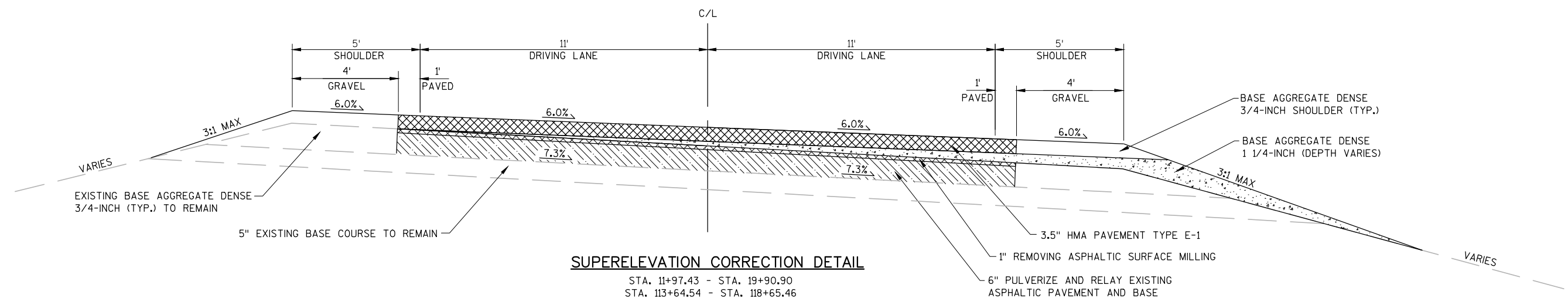


\* SEE TABLE FOR SUPERELEVATION  
 \*\* THE LOW SIDE SHOULDER SLOPE ON SUPERELEVATED SECTIONS EQUALS THE SUPERELEVATION WHEN THE SUPERELEVATION IS GREATER THAN 0.04 FT./FT. IF THE SUPERELEVATION IS LESS THAN OR EQUALS 0.04 FT./FT., THEN THE LOW SIDE SHOULDER SLOPE IS 0.04 FT./FT. THE HIGH SIDE SHOULDER SLOPE ON THE SUPERELEVATED SECTION EQUALS THE SUPERELEVATION.



- STA. 13+16.43 - STA. 18+71.90
- STA. 22+62.79 - STA. 29+54.82
- STA. 54+61.06 - STA. 61+34.78
- STA. 79+97.88 - STA. 86+77.18
- STA. 92+01.86 - STA. 97+23.33
- STA. 114+41.88 - STA. 117+88.12
- STA. 128+34.54 - STA. 136+01.88
- STA. 145+48.48 - STA. 153+44.18
- STA. 168+35.78 - STA. 176+62.00
- STA. 179+52.06 - STA. 184+22.51
- STA. 192+52.04 - STA. 196+67.41

Date	Revision

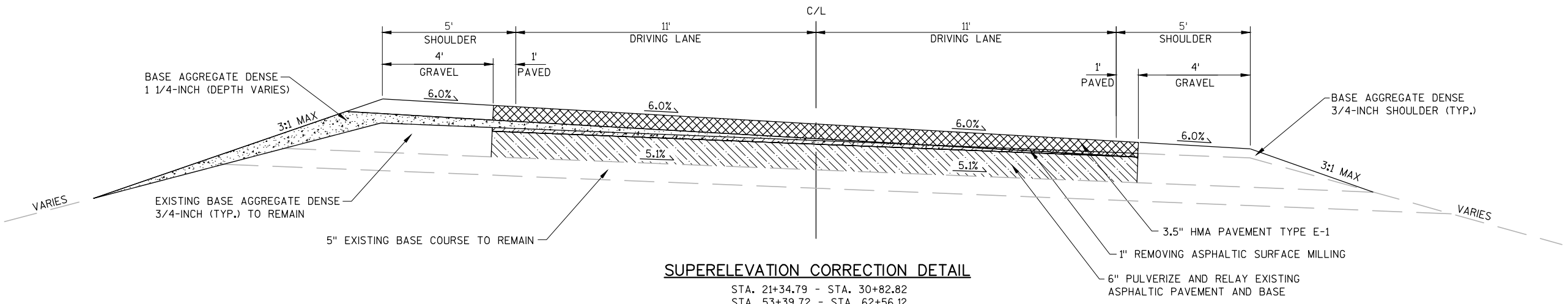


**SUPERELEVATION CORRECTION DETAIL**

- STA. 11+97.43 - STA. 19+90.90
- STA. 113+64.54 - STA. 118+65.46
- STA. 127+26.87 - STA. 137+09.55
- STA. 178+32.73 - STA. 185+41.84
- \*STA. 211+57.58 - STA. 219+26.43

\*SHOULDER WIDTH VARIES FROM 1' PAVED TO 3' PAVED

NOTE: DETAIL ABOVE SHOWS CORRECTION OF SUPERELEVATION IN CURVE 1 FROM AN EXISTING SUPERELEVATION OF 7.3% TO A PROPOSED SUPERELEVATION OF 6.0%.



**SUPERELEVATION CORRECTION DETAIL**

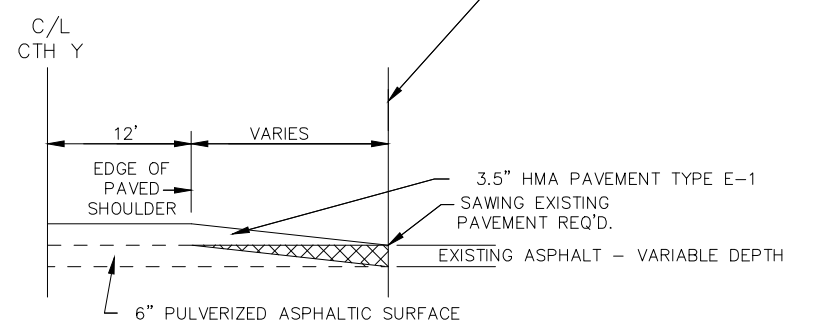
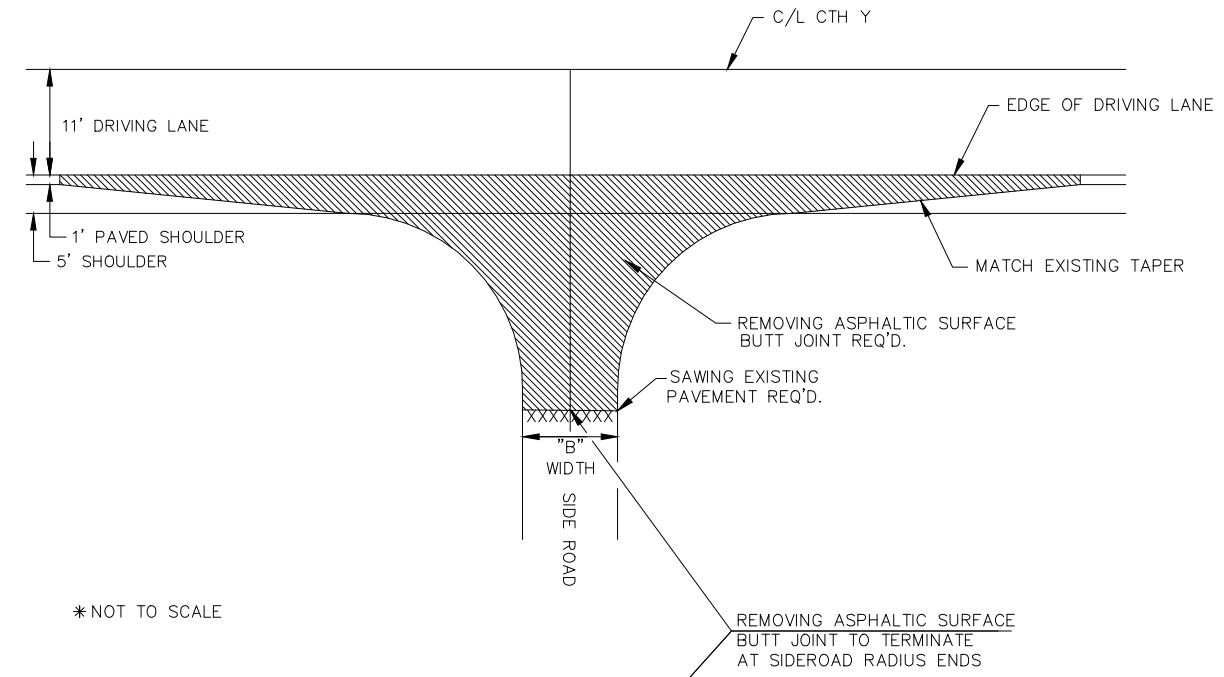
- STA. 21+34.79 - STA. 30+82.82
- STA. 53+39.72 - STA. 62+56.12
- STA. 79+03.88 - STA. 87+71.18
- STA. 90+82.52 - STA. 98+42.67
- STA. 144+27.15 - STA. 154+65.51
- STA. 167+07.78 - STA. 177+90.00
- \*STA. 191+32.71 - STA. 199+58.23
- \*STA. 199+51.70 - STA. 207+64.28
- \*STA. 232+10.34 - STA. 245+47.14

\*SHOULDER WIDTH VARIES FROM 1' PAVED TO 3' PAVED

NOTE: DETAIL ABOVE SHOWS CORRECTION OF SUPERELEVATION IN CURVE 9 FROM AN EXISTING SUPERELEVATION OF 5.1% TO A PROPOSED SUPERELEVATION OF 6.0%.

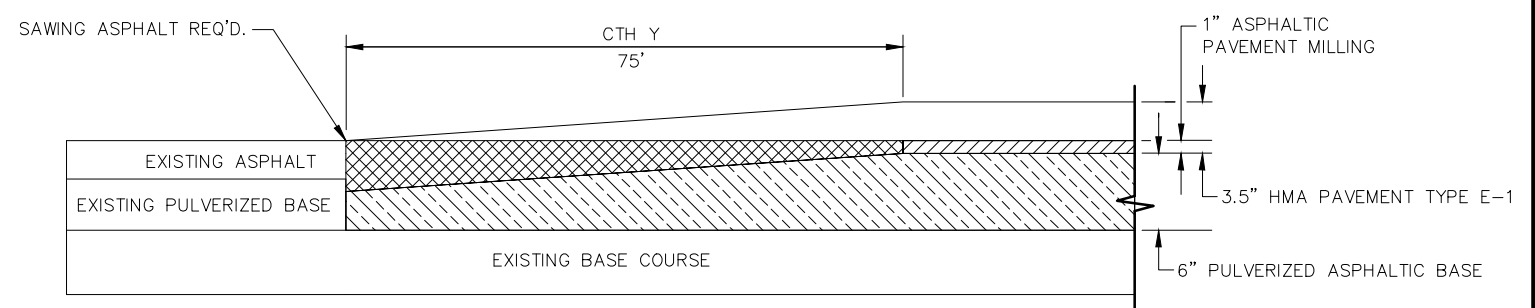
**SIDEROAD DATA TABLE**

SIDEROAD	LOCATION	TYPE	MATCH EX. AT	"B" WIDTH
OLD COUNTY RD Y/PALACE STREET	MAINLINE, RT	ASPHALT	40' RT	24'
MISSION LAKE ROAD	MAINLINE, RT	ASPHALT	60' RT	22'
KRISTOF ROAD	MAINLINE, LT	ASPHALT	60' LT	22'
NORMANS ROAD (SOUTH)	MAINLINE, RT	ASPHALT	35' RT	20'
NORMANS ROAD (NORTH)	MAINLINE, LT	B.A.D.	35' LT	20'
SHADY LANE (EAST)	MAINLINE, RT	ASPHALT	40' RT	22'
SHADY LANE (WEST)	MAINLINE, LT	ASPHALT	50' LT	22'
BALLROOM LANE	MAINLINE, RT	ASPHALT	40' RT	20'
N. SHORE DRIVE (STA. 222+70.50)	MAINLINE, RT	ASPHALT	45' RT	20'
N. SHORE DRIVE (STA. 240+38.42)	MAINLINE, RT	ASPHALT	50' RT	20'
PLQVER RIVER ROAD	MAINLINE, LT	ASPHALT	60' LT	22'
WOODRA ACRES ROAD	MAINLINE, RT	ASPHALT	45' RT	22'
CASANDRAM JO LANE	MAINLINE, LT	ASPHALT	45' LT	22'
COUNTY ROAD II	MAINLINE, RT	ASPHALT	90' RT	22'
BRIDGE ROAD	MAINLINE, LT	ASPHALT	40' LT	22'



**SIDEROAD DETAIL - NO CURB & GUTTER**

NOTE: IF THE EXISTING SIDEROAD CONSISTS OF A BASE COURSE SURFACE, PLACE 2" HMA PAVEMENT TYPE E-1 FROM THE EDGE OF THE PAVED SHOULDER ON CTH Y TO THE ENDS OF THE EXISTING SIDEROAD RADIUS. PLACE ADDITIONAL BASE AGGREGATE DENSE TO MATCH EXISTING SURFACES.



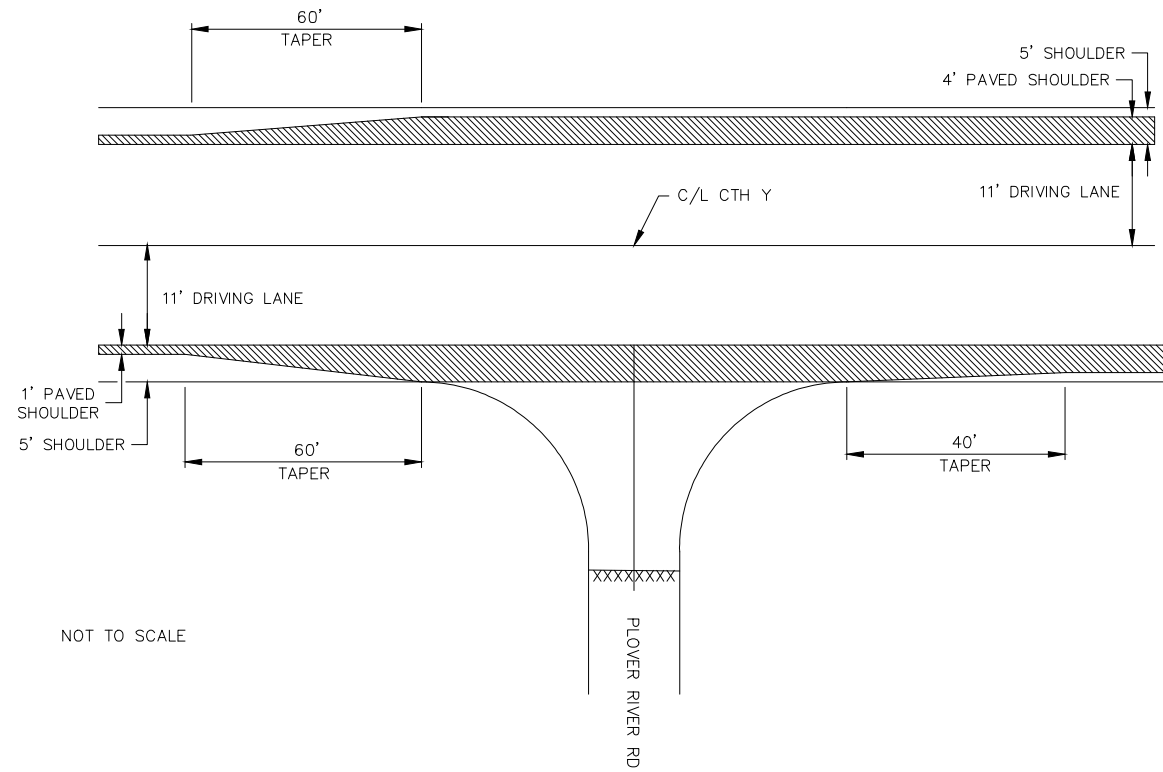
NOTE: REQUIRED AT BEGIN AND END OF PAVING LOCATIONS

PAID FOR AS REMOVING ASPHALTIC SURFACE BUTT JOINTS  
 PAID FOR AS PULVERIZE AND RELAY  
 PAID FOR AS REMOVING ASPHALTIC PAVEMENT MILLING

**DETAILS OF TRANSVERSE JOINT**

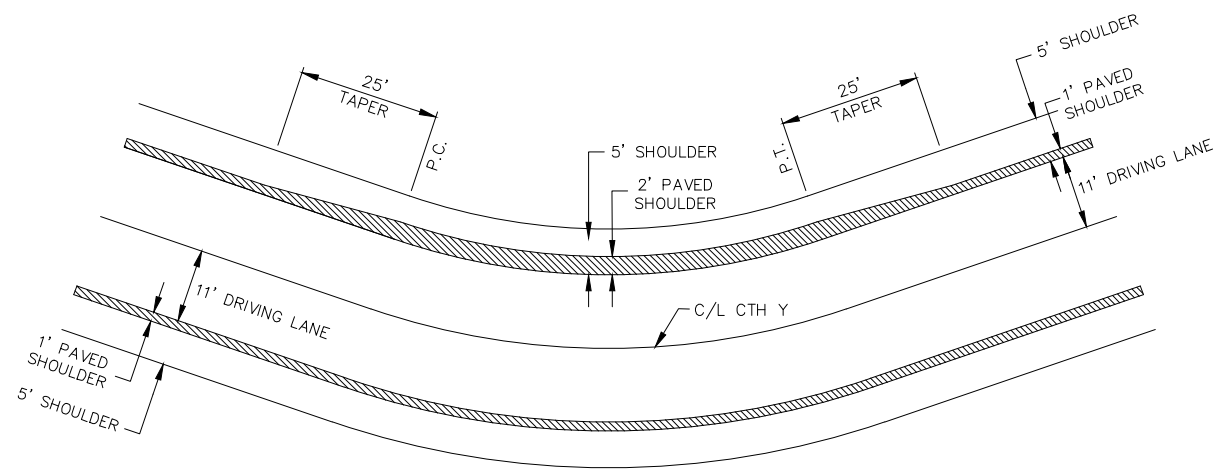
STA. 10+50 - STA. 11+25  
 STA. 366+87 - STA. 367+62

NORTH



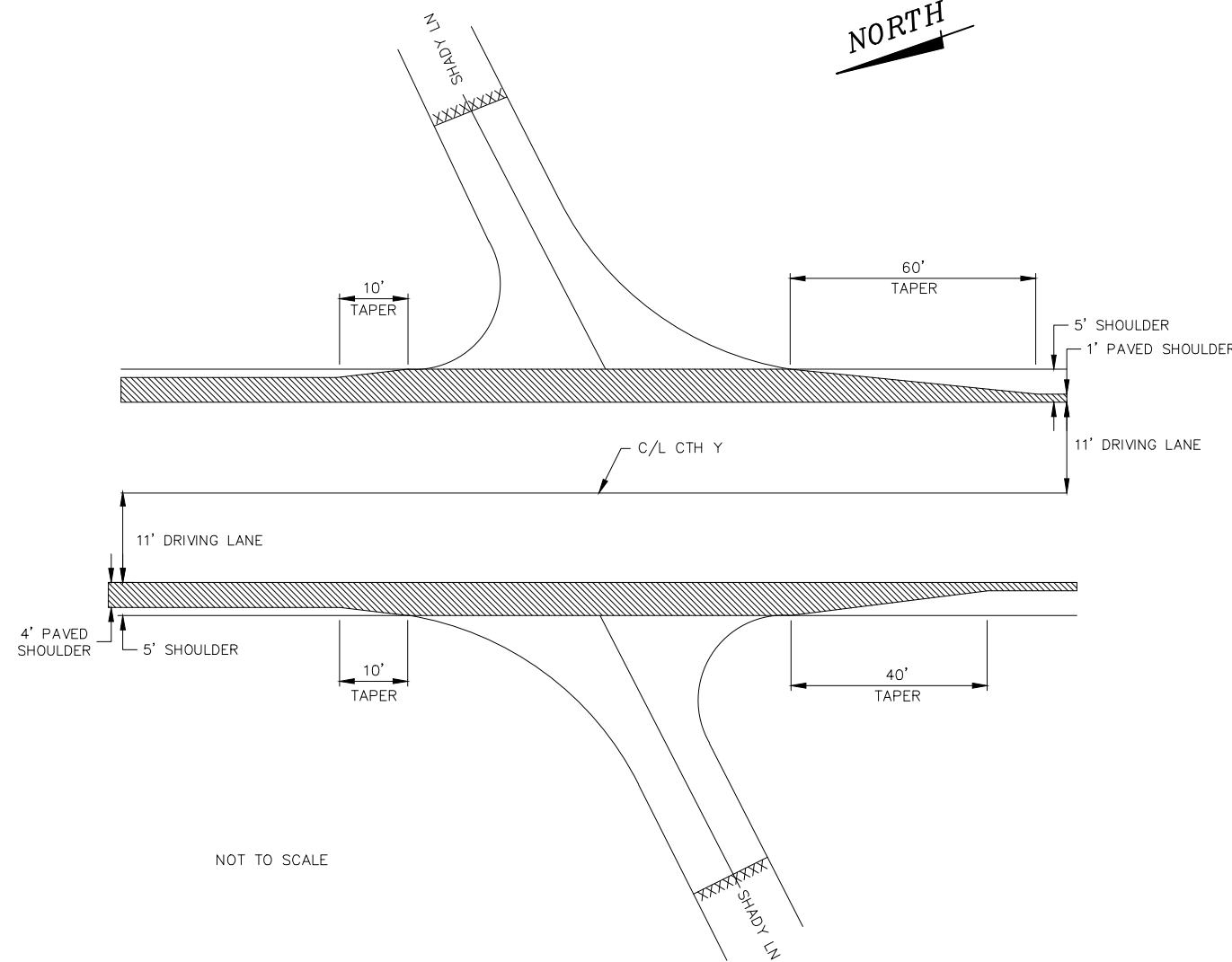
TAPER DETAIL FOR 4' PAVED SHOULDER SECTION AT PLOVER RIVER ROAD INTERSECTION

NOT TO SCALE



1' PAVEMENT WIDENING TAPER DETAIL AT CURVE SECTIONS

NORTH



TAPER DETAIL FOR 4' PAVED SHOULDER SECTION AT SHADY LANE INTERSECTION

NOT TO SCALE

**JEWELL**  
associates engineers, inc.  
Engineers - Planners - Surveyors

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**CTH Y**  
**STH 153 - CTH II**  
**MARATHON COUNTY, WISCONSIN**

Date  
**2/28/2013**

Date Revision

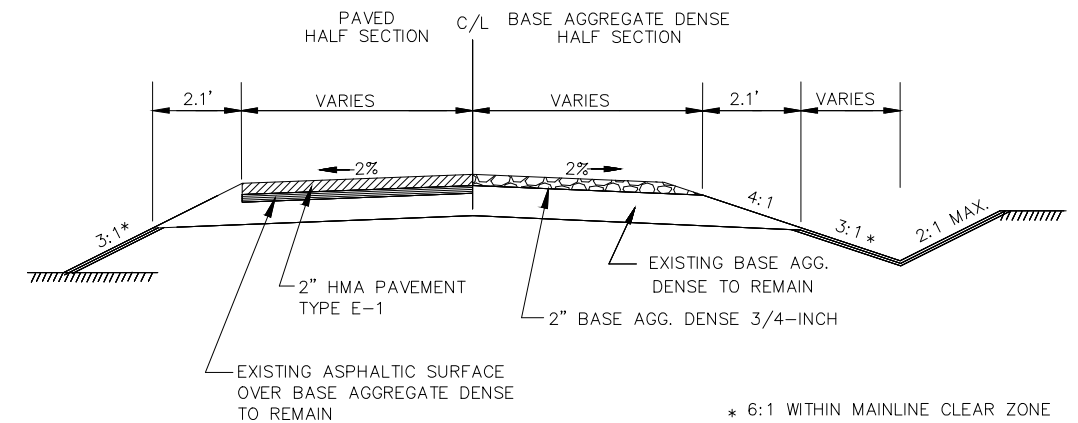
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**CONSTRUCTION  
DETAILS**

Sheet No.  
**C2.5**

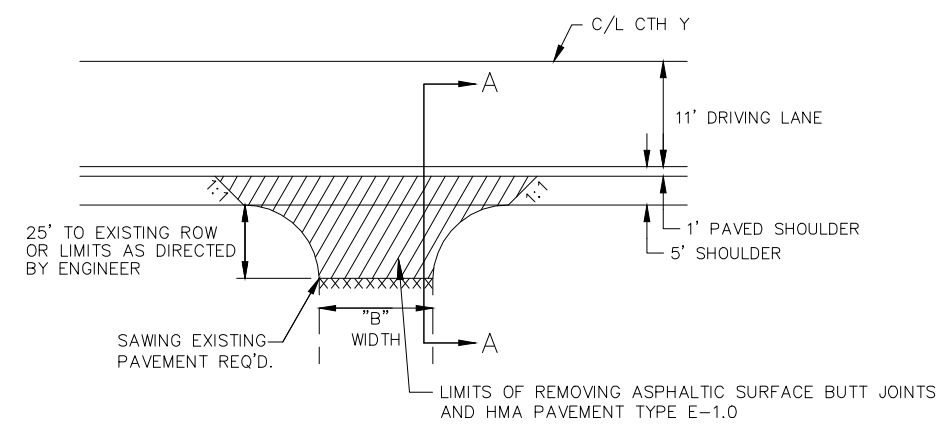
Jewell Project Number  
**M97020**

**DRIVEWAY INFORMATION**

STATION	LOCATION	TYPE	MATCH EX. AT	"B" WIDTH	STATION	LOCATION	TYPE	MATCH EX. AT	"B" WIDTH
10+84.20	MAINLINE, RT	B.A.D.	41' RT	12'	169+36.74	MAINLINE, LT	B.A.D.	41' LT	15'
14+40.11	MAINLINE, RT	B.A.D.	41' RT	12'	169+69.22	MAINLINE, RT	B.A.D.	41' RT	14'
22+61.80	MAINLINE, LT	B.A.D.	41' LT	15'	171+39.66	MAINLINE, LT	B.A.D.	41' LT	13'
32+46.91	MAINLINE, RT	B.A.D.	41' RT	29'	177+45.71	MAINLINE, RT	B.A.D.	41' RT	14'
35+50.00	MAINLINE, RT	B.A.D.	41' RT	13'	177+51.16	MAINLINE, LT	B.A.D.	41' LT	14'
35+80.05	MAINLINE, LT	B.A.D.	41' LT	16'	190+31.93	MAINLINE, RT	B.A.D.	41' RT	14'
37+55.85	MAINLINE, LT	ASPHALT	41' LT	14'	198+72.14	MAINLINE, RT	B.A.D.	41' RT	13'
40+96.66	MAINLINE, LT	ASPHALT	41' LT	15'	205+43.72	MAINLINE, LT	B.A.D.	41' LT	15'
42+81.16	MAINLINE, RT	B.A.D.	41' RT	18'	207+65.65	MAINLINE, RT	ASPHALT	41' RT	77'
43+88.69	MAINLINE, RT	B.A.D.	41' RT	20'	208+89.92	MAINLINE, LT	B.A.D.	41' LT	11'
44+55.52	MAINLINE, LT	B.A.D.	41' LT	18'	211+47.50	MAINLINE, RT	B.A.D.	41' RT	12'
45+14.50	MAINLINE, RT	B.A.D.	41' RT	20'	211+76.67	MAINLINE, LT	B.A.D.	41' LT	32'
47+91.40	MAINLINE, LT	ASPHALT	41' LT	17'	216+65.28	MAINLINE, LT	ASPHALT	41' LT	14'
49+84.03	MAINLINE, LT	ASPHALT	41' LT	12'	219+08.55	MAINLINE, RT	ASPHALT	41' RT	11'
51+22.87	MAINLINE, RT	B.A.D.	41' RT	16'	219+32.88	MAINLINE, RT	ASPHALT	41' RT	11'
51+90.00	MAINLINE, RT	B.A.D.	41' RT	16'	220+13.32	MAINLINE, RT	ASPHALT	41' RT	32'
53+14.24	MAINLINE, LT	ASPHALT	41' LT	14'	221+25.32	MAINLINE, RT	ASPHALT	41' RT	18'
55+75.20	MAINLINE, LT	ASPHALT	41' LT	20'	221+32.14	MAINLINE, LT	ASPHALT	41' LT	22'
56+37.82	MAINLINE, LT	B.A.D.	41' LT	14'	223+66.83	MAINLINE, LT	ASPHALT	41' LT	13'
57+22.28	MAINLINE, RT	ASPHALT	41' RT	12'	228+27.82	MAINLINE, LT	B.A.D.	41' LT	20'
57+96.63	MAINLINE, LT	ASPHALT	41' LT	22'	228+27.82	MAINLINE, RT	B.A.D.	41' RT	19'
64+44.91	MAINLINE, RT	B.A.D.	41' RT	22'	231+25.55	MAINLINE, RT	B.A.D.	41' RT	16'
67+89.68	MAINLINE, LT	B.A.D.	41' LT	15'	234+66.40	MAINLINE, RT	B.A.D.	41' RT	13'
69+15.05	MAINLINE, RT	ASPHALT	41' RT	20'	237+88.37	MAINLINE, LT	B.A.D.	41' LT	12'
80+77.03	MAINLINE, RT	B.A.D.	41' RT	14'	238+20.64	MAINLINE, RT	ASPHALT	41' RT	18'
91+34.09	MAINLINE, LT	B.A.D.	41' LT	41'	241+89.72	MAINLINE, LT	B.A.D.	41' LT	17'
91+66.22	MAINLINE, RT	B.A.D.	41' RT	13'	243+72.62	MAINLINE, LT	B.A.D.	41' LT	22'
100+99.80	MAINLINE, RT	B.A.D.	41' RT	15'	248+24.05	MAINLINE, RT	B.A.D.	41' RT	28'
101+24.11	MAINLINE, LT	B.A.D.	41' LT	13'	248+54.43	MAINLINE, LT	B.A.D.	41' LT	32'
110+52.62	MAINLINE, RT	B.A.D.	41' RT	16'	256+11.96	MAINLINE, LT	B.A.D.	41' LT	16'
111+55.89	MAINLINE, LT	B.A.D.	41' LT	14'	258+18.26	MAINLINE, LT	ASPHALT	41' LT	14'
118+39.21	MAINLINE, RT	B.A.D.	41' RT	15'	260+31.18	MAINLINE, RT	B.A.D.	41' RT	30'
119+49.50	MAINLINE, LT	B.A.D.	41' LT	20'	261+70.87	MAINLINE, LT	ASPHALT	41' LT	18'
120+76.95	MAINLINE, RT	ASPHALT	41' RT	13'	265+06.40	MAINLINE, LT	ASPHALT	41' LT	23'
124+77.08	MAINLINE, RT	B.A.D.	41' RT	14'	270+07.00	MAINLINE, LT	B.A.D.	41' LT	15'
128+10.43	MAINLINE, RT	ASPHALT	41' RT	12'	272+16.10	MAINLINE, RT	B.A.D.	41' RT	19'
130+69.30	MAINLINE, RT	B.A.D.	41' RT	19'	278+58.43	MAINLINE, LT	B.A.D.	41' LT	17'
131+33.70	MAINLINE, RT	B.A.D.	41' RT	18'	282+31.90	MAINLINE, LT	B.A.D.	41' LT	14'
137+01.80	MAINLINE, RT	B.A.D.	41' RT	20'	286+98.13	MAINLINE, LT	B.A.D.	41' LT	41'
139+26.24	MAINLINE, LT	B.A.D.	41' LT	15'	291+23.42	MAINLINE, LT	ASPHALT	41' LT	12'
139+51.50	MAINLINE, RT	B.A.D.	41' RT	11'	291+64.58	MAINLINE, RT	B.A.D.	41' RT	19'
140+72.90	MAINLINE, LT	B.A.D.	41' LT	12'	292+39.87	MAINLINE, LT	B.A.D.	41' LT	10'
144+34.97	MAINLINE, RT	B.A.D.	41' RT	18'	293+40.28	MAINLINE, LT	B.A.D.	41' LT	24'
145+68.44	MAINLINE, RT	B.A.D.	41' RT	13'	296+83.02	MAINLINE, LT	ASPHALT	41' LT	18'
145+71.75	MAINLINE, LT	B.A.D.	41' LT	16'	298+97.34	MAINLINE, LT	ASPHALT	41' LT	17'
148+10.19	MAINLINE, LT	B.A.D.	41' LT	14'	302+20.57	MAINLINE, LT	ASPHALT	41' LT	22'
150+56.22	MAINLINE, LT	B.A.D.	41' LT	18'	304+72.79	MAINLINE, LT	B.A.D.	41' LT	16'
152+25.07	MAINLINE, LT	B.A.D.	41' LT	16'					

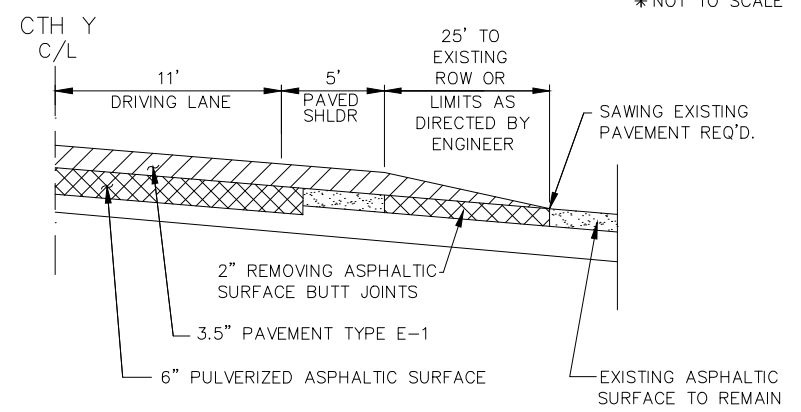


**TYPICAL SECTION FOR PRIVATE ENTRANCES**



ANY ADDITIONAL BASE AGG. DENSE REQ'D. SHALL BE PAID UNDER ITEM - "BASE AGGREGATE DENSE 3/4-INCH"

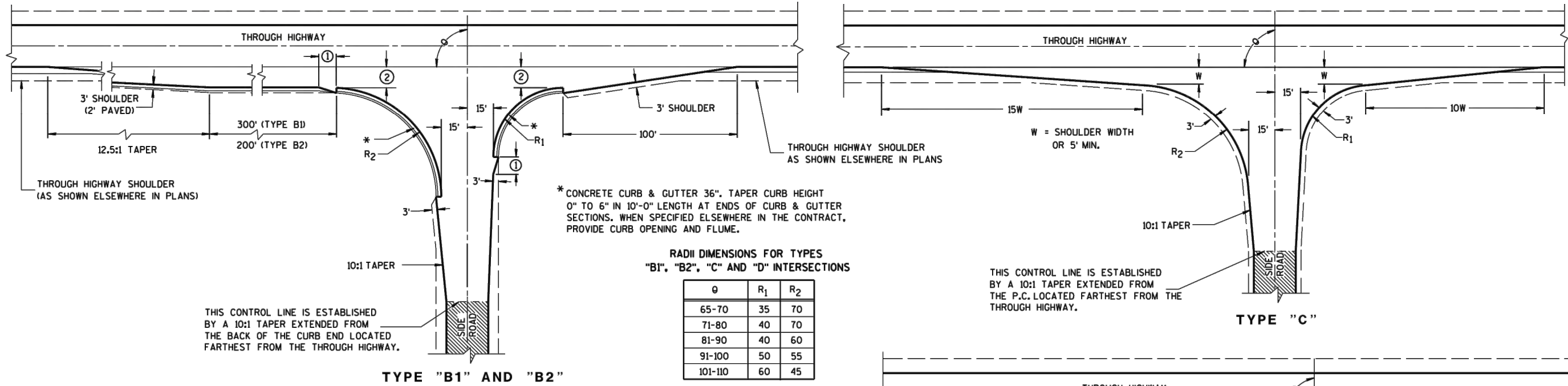
**RURAL DRIVEWAY DETAIL - ASPHALT**



**SECTION A-A**



# SIDEROAD INTERSECTION DETAIL



## GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

### SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

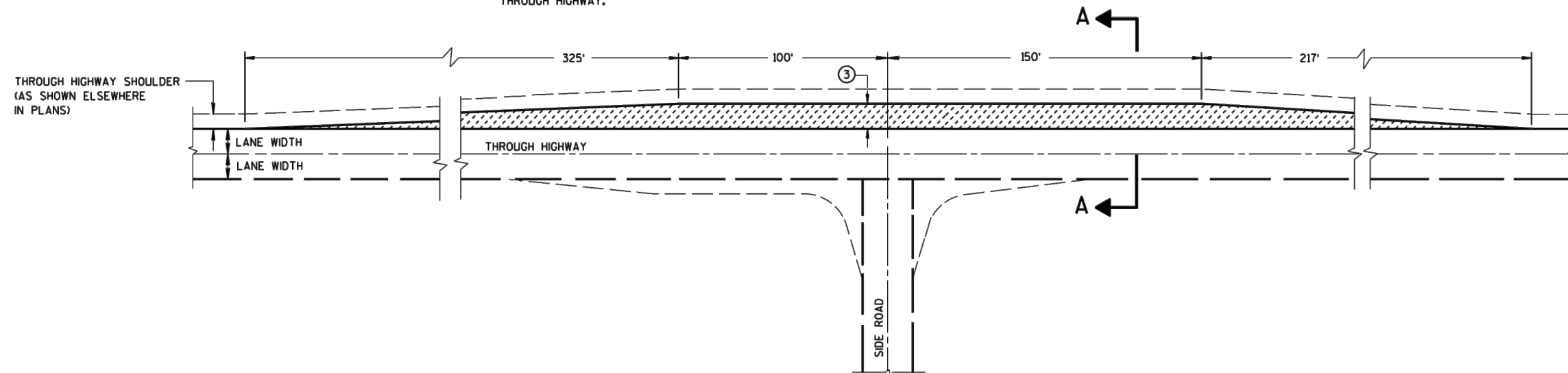
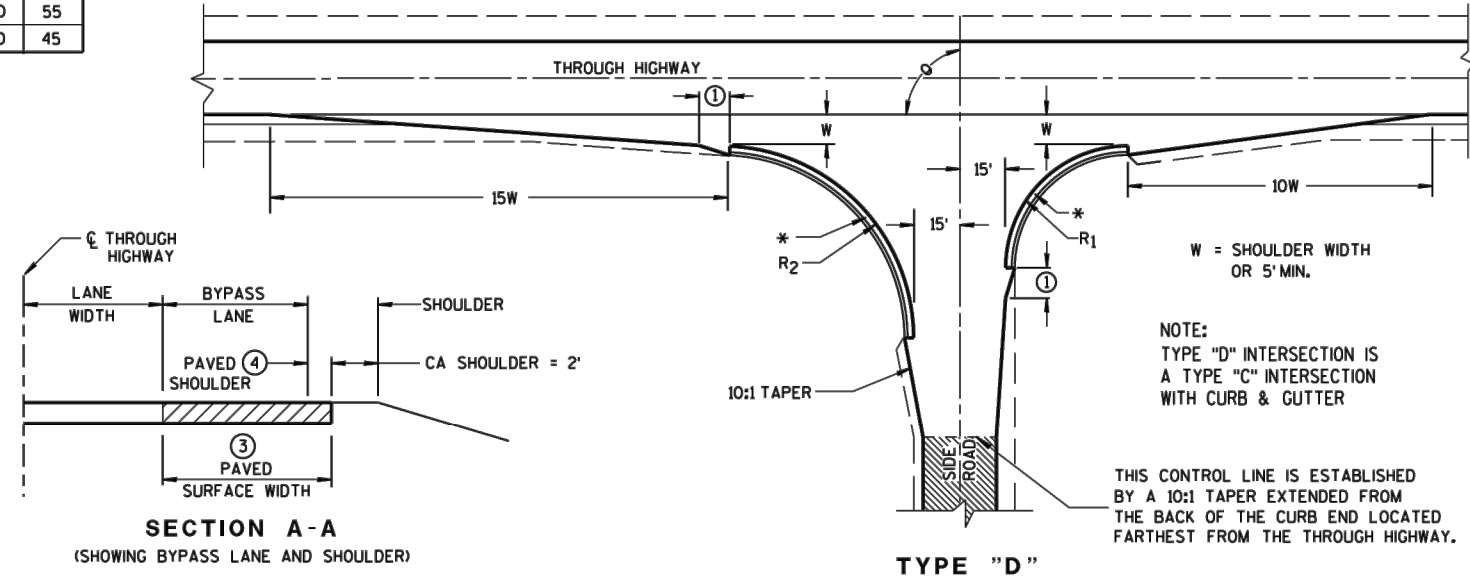
WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

EXISTING PAVED SURFACE

BYPASS LANE

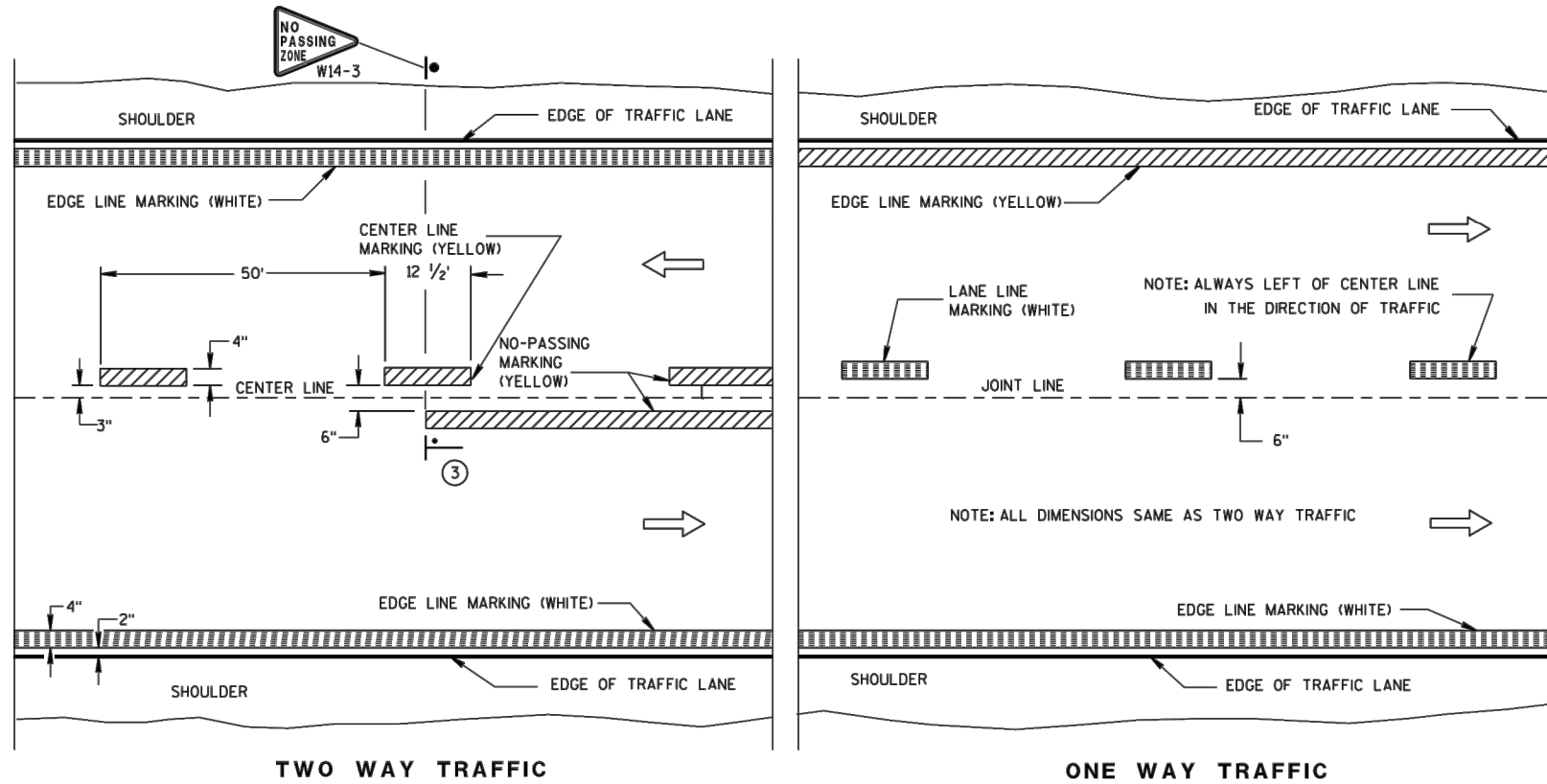
- ① 10-FT TYPICAL.
- ② 12-FT\*\* PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.  
\*\*10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.
- ③ BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE  
- ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH.  
- PC CONCRETE = 13-FT PLUS PAVED SHOULDER WIDTH.
- ④ BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.



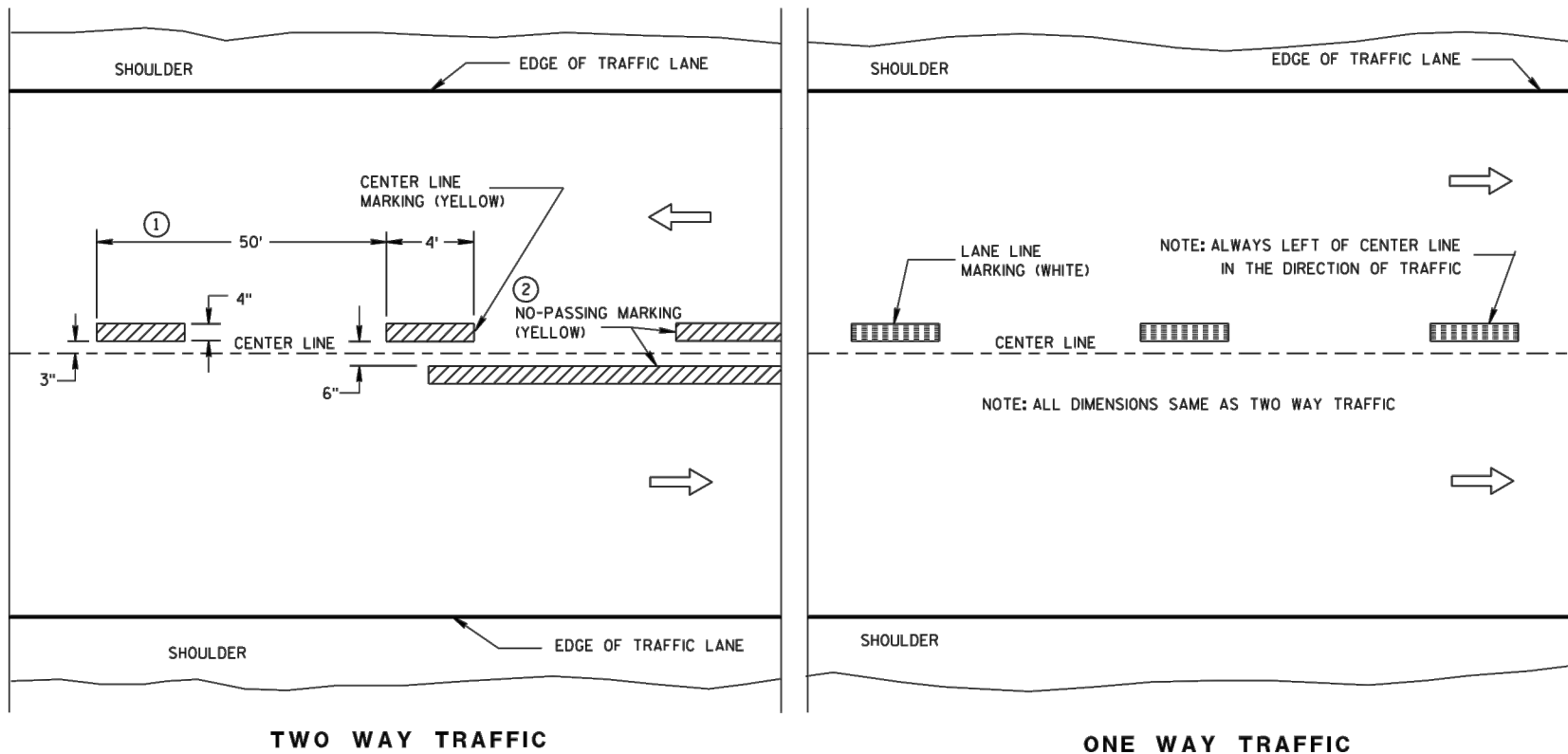
AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND "D" AND TEE INTERSECTION BYPASS LANE

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

# PAVEMENT MARKING (MAINLINE) DETAIL



## PERMANENT PAVEMENT MARKING



## TEMPORARY (INTERMEDIATE) PAVEMENT MARKING (SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

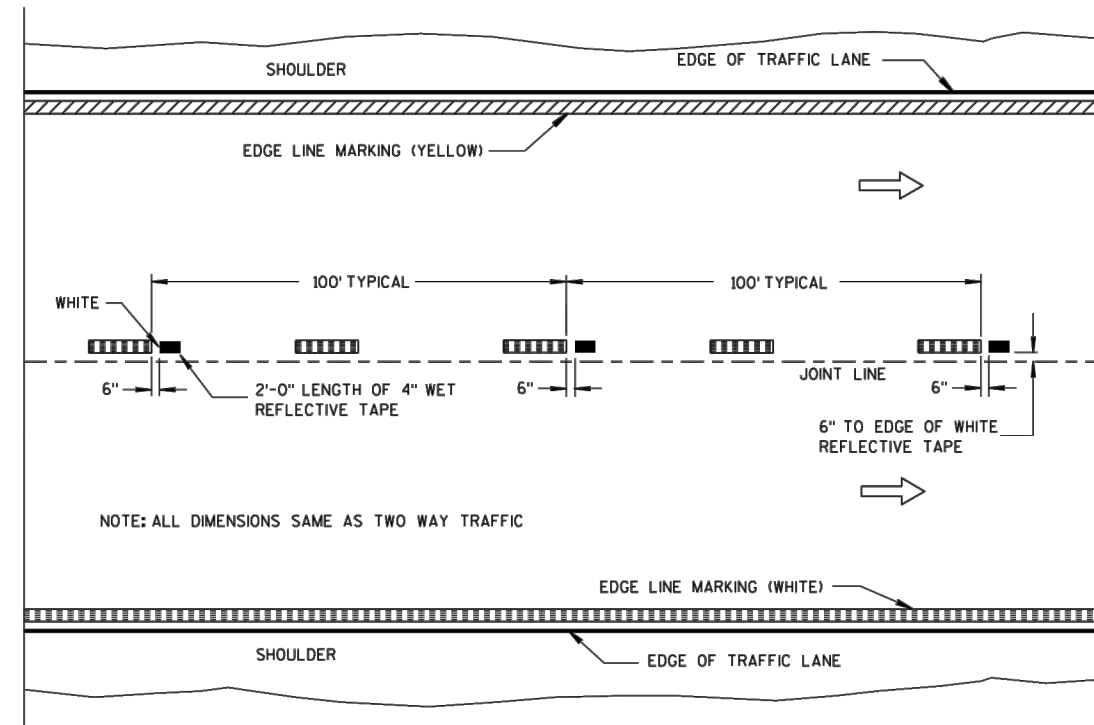
### GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- HALF CYCLE LENGTHS (25'+) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.

### NOTE

ARROW SYMBOL ( → ) SHOWS DIRECTION OF TRAVEL



## WET REFLECTIVE TAPE SUPPLEMENT TO SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

### LEGEND

- "T" MARKING
- POST MOUNTED SIGN

### PAVEMENT MARKING (MAINLINE)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
6-23-11 /S/ Thomas N. Notbohm  
DATE STATE TRAFFIC ENGINEER OF DESIGN

**JEWELL**  
associates engineers, inc.  
Engineers - Planners - Surveyors

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CTH Y  
STH 153 - CTH II  
MARATHON COUNTY, WISCONSIN

Date  
2/28/2013

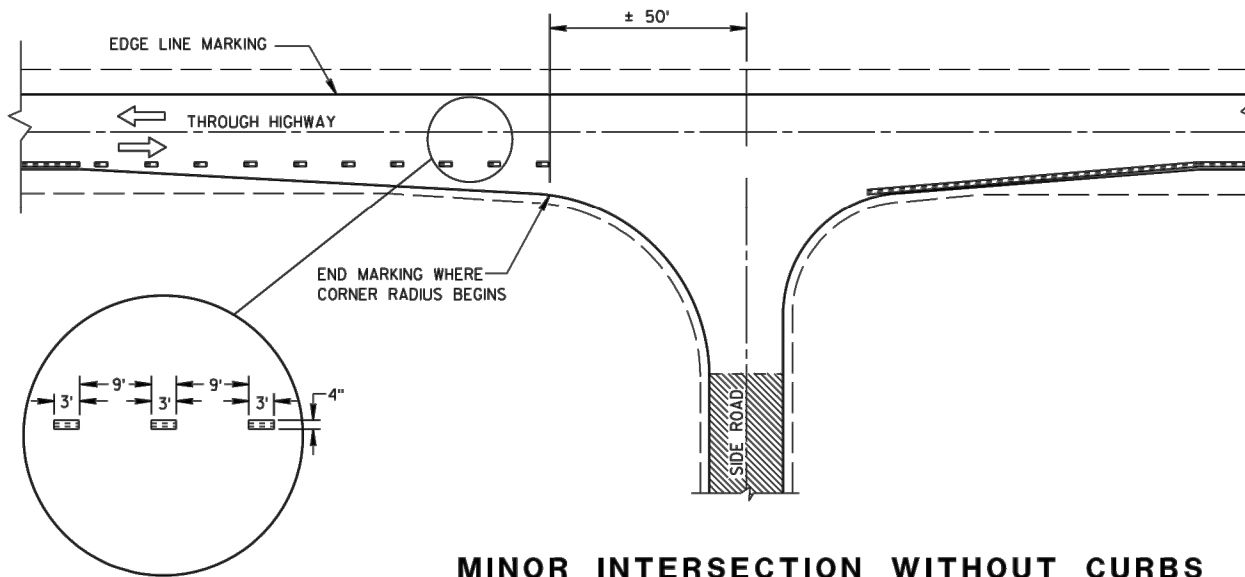
Date Revision

Drawing Name  
CONSTRUCTION  
DETAILS

Sheet No.  
C2.8

Jewell Project Number  
M97020

# PAVEMENT MARKING (INTERSECTIONS) DETAIL



**MINOR INTERSECTION WITHOUT CURBS**

⑦

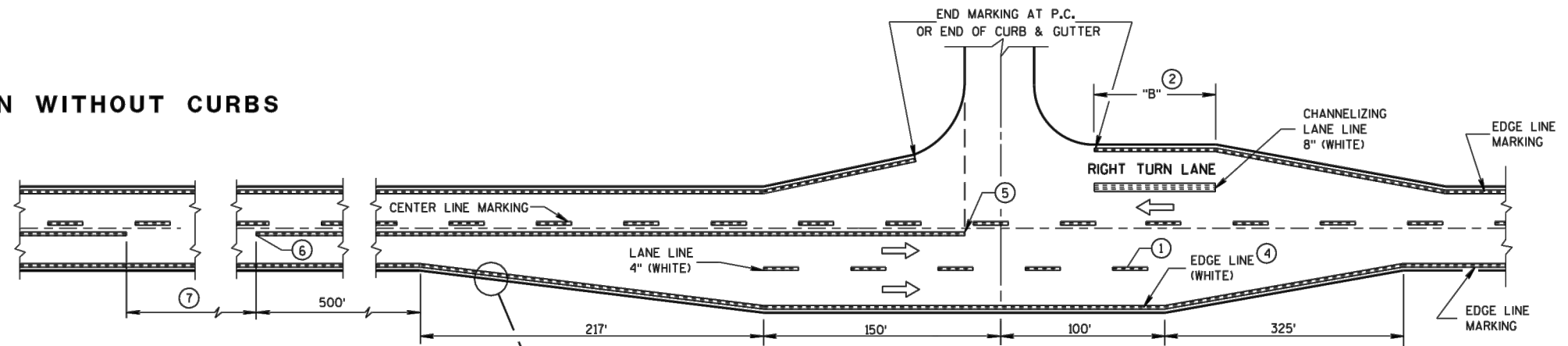
POSTED SPEED (MPH)	MINIMUM DISTANCE BETWEEN ZONES (FEET)
25 - 30	528
35 - 40	528
45 - 50	686
55	792

## GENERAL NOTES

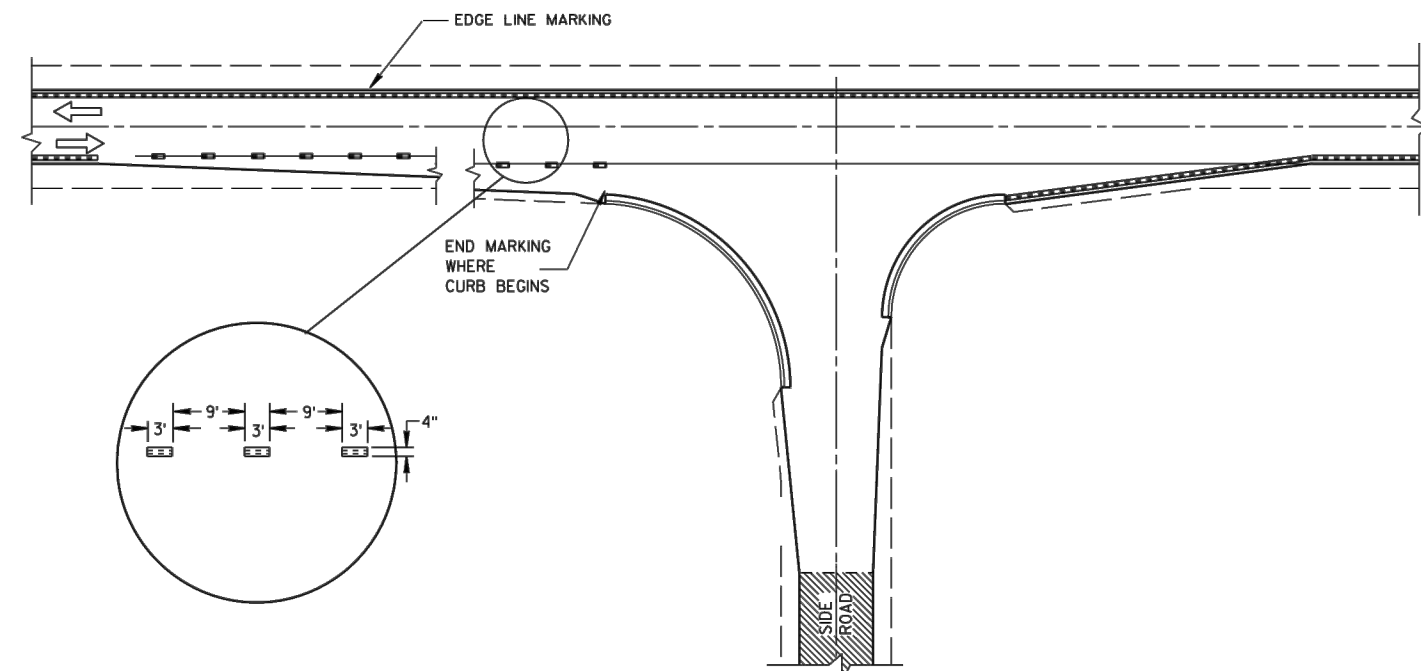
- EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
  - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
  - ③ ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.
  - ④ THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.

- ⑤ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
- ⑥ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑦ IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.
- ⑧ 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.

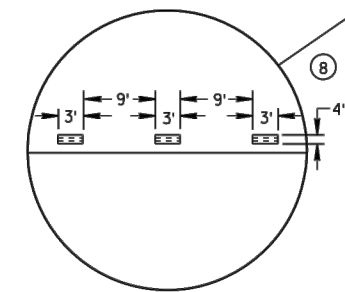
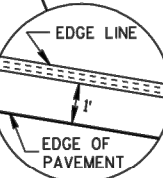
ARROW SYMBOL ( → ) SHOWS DIRECTION OF TRAVEL



**MAJOR INTERSECTIONS  
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)**



**MINOR INTERSECTION WITH CURBS  
(TYPICAL MARKING)**



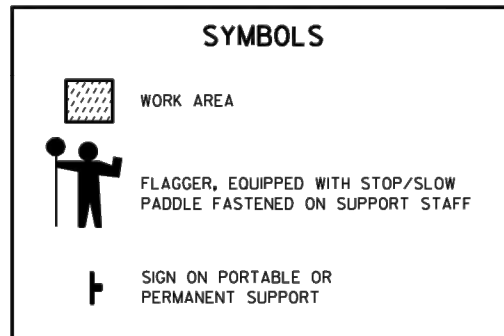
**MINOR INTERSECTION WITH CURBS  
③ (FOR SPECIAL CONDITIONS AS SPECIFIED)**

**PAVEMENT MARKING  
(INTERSECTIONS)**

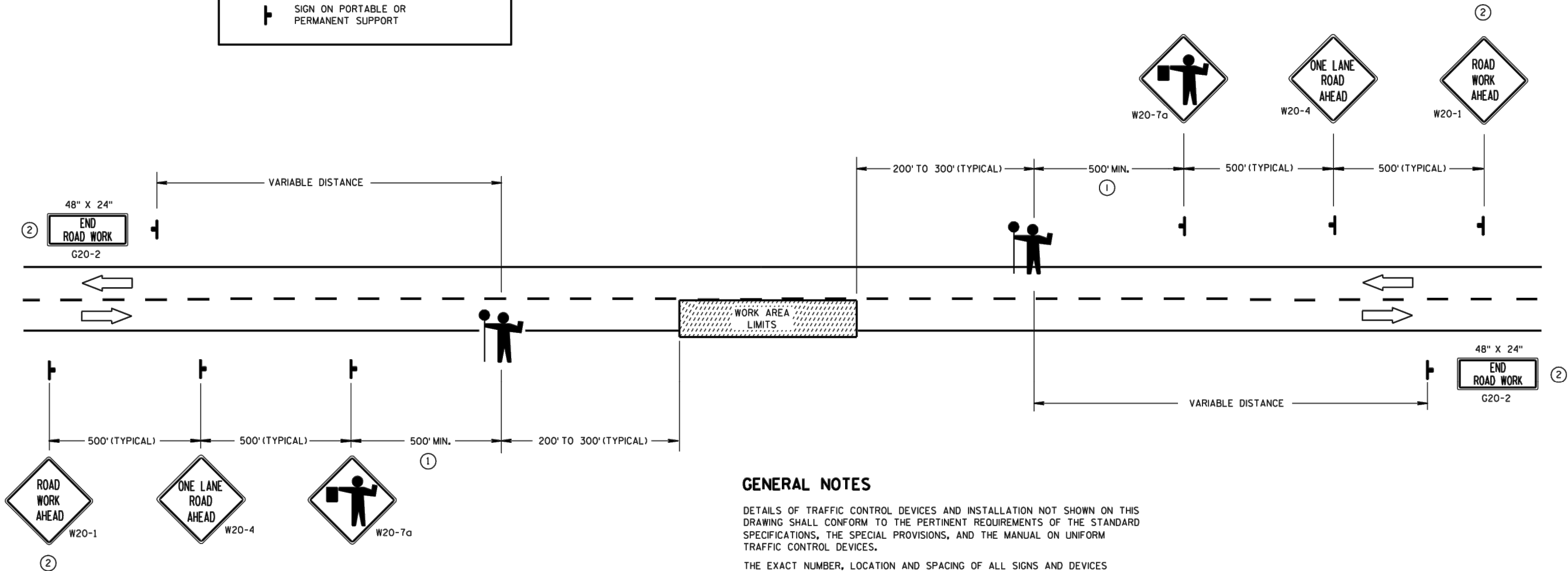
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

# TRAFFIC CONTROL MOVING OPERATIONS DETAIL

TWO-LANE ROADWAY



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

### GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, THE "FLAGGER AHEAD", THE "ROAD WORK AHEAD" AND THE ONE LANE ROAD AHEAD" SIGNS SHALL BE COVERED OR REMOVED AND THE HIGHWAY RESTORED TO NORMAL OPERATION.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

**JEWELL**  
 associates engineers, inc.  
 Engineers - Planners - Surveyors

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**CTH Y  
 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**

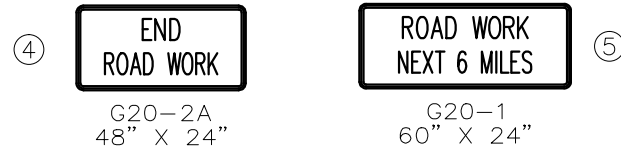
Date  
**2/28/2013**

Date Revision

Drawing Name  
**CONSTRUCTION  
 DETAILS**

<b>TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 9/5/06 /S/ Thomas N. Notbohm DATE STATE TRAFFIC ENGINEER OF DESIGN	FHWA

Sheet No.  
**C2.10**  
 Jewell Project Number  
**M97020**



**GENERAL NOTES**

THE EXACT LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

ALL SIGNS SHALL BE 48" x 48" UNLESS OTHERWISE NOTED.

ALL ROADS AND STREETS WITHIN THE WORK ZONES SHALL BE KEPT ACCESSIBLE FOR EMERGENCY VEHICLES, RESIDENTS AND BUSINESSES.

ANY STOP SIGNS WHICH ARE REMOVED FOR A CONSTRUCTION OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED.

ALL SIGN LAYOUT SHALL BE IN ACCORDANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL OF STANDARD HIGHWAY SIGNS.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AND MAINTAIN ACCESS TO ALL PROPERTY ABUTTING THE ROADWAY CONSTRUCTION WORK THROUGHOUT THE LIFE OF THE PROJECT.

THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL NECESSARY BARRICADES, SIGNS, LIGHTS, TEMPORARY MARKINGS, FLAGGERS, AND SUCH OTHER SAFETY DEVICES AS CALLED FOR ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

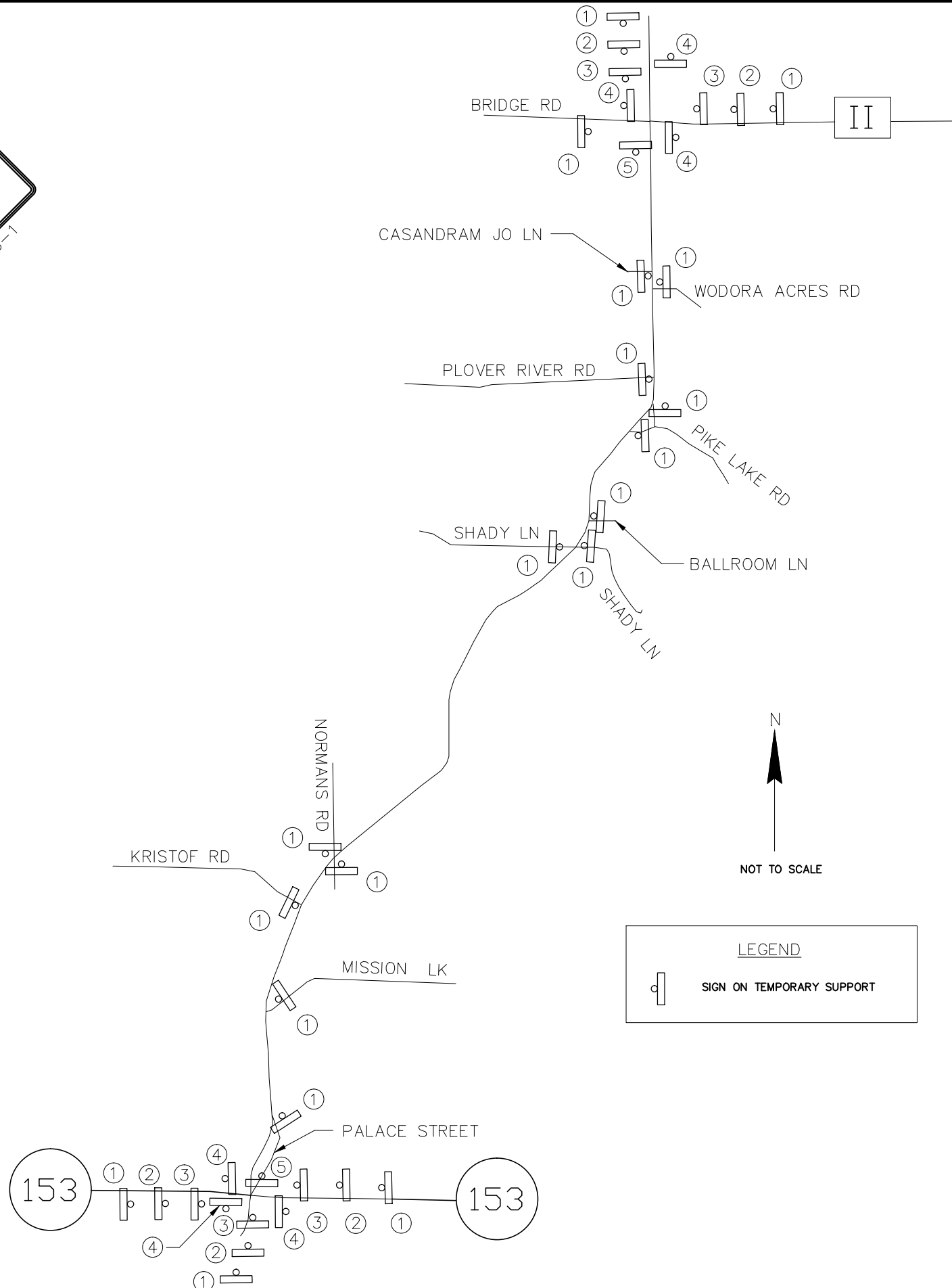
THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200' CLEARANCE TO EXISTING SIGNS.

DURING NIGHT SHUTDOWN, ONE LANE IN EACH DIRECTION MUST REMAIN OPEN. (ON A HARD SURFACE FOR MAINLINE, OR BASE AGGREGATE DENSE FOR SIDE ROADS).

DRAWINGS SHOW TRAFFIC CONTROL FOR A TYPICAL SITUATION. ADDITIONAL TRAFFIC CONTROL DEVICES MAY BE REQUIRED AND/OR LAYOUT DETAILS MODIFIED DEPENDING ON CONTRACTOR'S METHODS OR SEQUENCES OF OPERATION.

ROAD MACHINERY, FLAGGERS AHEAD, ETC. SIGNS SHALL BE USED AS NEEDED AND SHALL BE REMOVED OR COVERED AT NIGHTS, ON WEEKENDS OR WHEN THE ACTIVITY DOES NOT EXIST.

SIGN PLACEMENT SHALL FOLLOW SDD, "TRAFFIC CONTROL ADVANCE WARNINGS SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC" UNLESS OTHERWISE NOTED IN THE TRAFFIC CONTROL PLAN.






**CTH Y - CURVE 9**

STATION	LEFT	RIGHT	COMMENT
167+07.78	2.00% ←	2.00% →	END NORMAL CROWN - BEGIN SUPER
167+50	0.34% ←	2.00% →	
167+58.78	0.00% ←	2.00% →	
168+00	1.62% →	2.00% →	
168+09.78	2.00% →	2.00% ←	RC
168+50	3.58% →	3.58% →	
168+60.78	4.00% →	4.00% →	P.C. STATION
169+00	5.54% →	5.54% →	
169+11.78	6.00% →	6.00% →	BEGIN FULL SUPER
169+50	6.00% →	6.00% →	
170+00	6.00% →	6.00% →	
170+50	6.00% →	6.00% →	
171+00	6.00% →	6.00% →	
171+50	6.00% →	6.00% →	
172+00	6.00% →	6.00% →	
172+50	6.00% →	6.00% →	
173+00	6.00% →	6.00% →	
173+50	6.00% →	6.00% →	
174+00	6.00% →	6.00% →	
174+50	6.00% →	6.00% →	
175+00	6.00% →	6.00% →	
175+50	6.00% →	6.00% →	
175+86.00	6.00% →	6.00% →	END FULL SUPER
176+00	5.45% →	5.45% →	
176+37.00	4.00% →	4.00% →	P.T. STATION
176+50	3.49% →	3.49% →	
176+88.00	2.00% →	2.00% →	RC
177+00	1.53% →	2.00% →	
177+39.00	0.00% →	2.00% →	
177+50	0.43% ←	2.00% →	
177+90.00	2.00% ←	2.00% →	BEGIN NORMAL CROWN - END SUPER

**CTH Y - CURVE 10**

STATION	LEFT	RIGHT	COMMENT
178+32.73	2.00% ←	2.00% →	END NORMAL CROWN - BEGIN SUPER
178+50	2.00% ←	1.32% →	
178+83.73	2.00% ←	0.00% →	
179+00	2.00% ←	0.64% ←	
179+34.64	2.00% ←	2.00% ←	RC
179+50	2.60% ←	2.60% ←	
179+77.06	3.67% ←	3.67% ←	P.C. STATION
180+00.00	4.57% ←	4.57% ←	
180+23.73	5.50% ←	5.50% ←	BEGIN FULL SUPER
180+50	5.50% ←	5.50% ←	
181+00	5.50% ←	5.50% ←	
181+50	5.50% ←	5.50% ←	
182+00	5.50% ←	5.50% ←	
182+50	5.50% ←	5.50% ←	
183+00	5.50% ←	5.50% ←	
183+50	5.50% ←	5.50% ←	
183+50.84	5.50% ←	5.50% ←	END FULL SUPER
183+97.51	3.67% ←	3.67% ←	P.T. STATION
184+00	3.57% ←	3.57% ←	
184+39.93	2.00% ←	2.00% ←	RC
184+50	2.00% ←	1.60% ←	
184+90.84	2.00% ←	0.00% →	
185+00	2.00% ←	0.36% →	
185+41.84	2.00% ←	2.00% →	BEGIN NORMAL CROWN - END SUPER

**CTH Y - CURVE 11**

STATION	LEFT	RIGHT	COMMENT
191+32.71	2.00% ←	2.00% →	END NORMAL CROWN - BEGIN SUPER
191+50	2.00% ←	1.32% →	
191+83.71	2.00% ←	0.00% →	
192+00	2.00% ←	0.84% ←	
192+34.62	2.00% ←	2.00% ←	RC
192+50	2.60% ←	2.60% ←	
192+77.04	3.67% ←	3.67% ←	P.C. STATION
193+00	4.57% ←	4.57% ←	
193+23.71	5.50% ←	5.50% ←	BEGIN FULL SUPER
193+50	5.50% ←	5.50% ←	
194+00	5.50% ←	5.50% ←	
194+50	5.50% ←	5.50% ←	
195+00	5.50% ←	5.50% ←	
195+50	5.50% ←	5.50% ←	
196+00	5.50% ←	5.50% ←	
196+50	5.50% ←	5.50% ←	
197+00	5.50% ←	5.50% ←	
197+50	5.50% ←	5.50% ←	
197+67.23	5.50% ←	5.50% ←	END FULL SUPER
198+00	4.21% ←	4.21% ←	
198+13.89	3.67% ←	3.67% ←	P.T. STATION
198+50	2.25% ←	2.25% ←	
198+56.32	2.00% ←	2.00% ←	RC
199+00	2.00% ←	0.28% ←	
199+07.23	2.00% ←	0.00% →	
199+50	2.00% ←	1.68% →	
199+58.23	2.00% ←	2.00% →	BEGIN NORMAL CROWN - END SUPER

**CTH Y - CURVE 12**

STATION	LEFT	RIGHT	COMMENT
199+51.70	2.00% ←	2.00% →	END NORMAL CROWN - BEGIN SUPER
200+00	2.00% ←	0.11% →	
200+02.70	2.00% ←	0.00% →	
200+50	2.00% ←	1.85% →	
200+53.70	2.00% ←	2.00% ←	RC
201+00	3.82% ←	3.82% ←	
201+04.70	4.00% ←	4.00% ←	P.C. STATION
201+50	5.78% ←	5.78% ←	
201+55.70	6.00% ←	6.00% ←	BEGIN FULL SUPER
202+00	6.00% ←	6.00% ←	
202+50	6.00% ←	6.00% ←	
203+00	6.00% ←	6.00% ←	
203+50	6.00% ←	6.00% ←	
204+00	6.00% ←	6.00% ←	
204+50	6.00% ←	6.00% ←	
205+00	6.00% ←	6.00% ←	
205+50	6.00% ←	6.00% ←	
205+60.28	6.00% ←	6.00% ←	END FULL SUPER
206+00	4.44% ←	4.44% ←	
206+11.27	4.00% ←	4.00% ←	P.T. STATION
206+50	2.48% ←	2.48% ←	
206+62.28	2.00% ←	2.00% ←	RC
207+00	2.00% ←	0.52% ←	
207+13.28	2.00% ←	0.00% →	
207+50	2.00% ←	1.44% →	
207+64.28	2.00% ←	2.00% →	BEGIN NORMAL CROWN - END SUPER

**CTH Y - CURVE 14**

STATION	LEFT	RIGHT	COMMENT
232+10.34	2.00% ←	2.00% →	END NORMAL CROWN - BEGIN SUPER
232+50	2.00% ←	0.44% →	
232+61.34	2.00% ←	0.00% →	
233+00	2.00% ←	1.51% ←	
233+12.57	2.00% ←	2.00% ←	RC
233+50	3.46% ←	3.46% ←	
233+58.68	3.80% ←	3.80% ←	P.C. STATION
234+00	5.41% ←	5.41% ←	
234+07.34	5.70% ←	5.70% ←	BEGIN FULL SUPER
234+50	5.70% ←	5.70% ←	
235+00	5.70% ←	5.70% ←	
235+50	5.70% ←	5.70% ←	
236+00	5.70% ←	5.70% ←	
236+50	5.70% ←	5.70% ←	
237+00	5.70% ←	5.70% ←	
237+50	5.70% ←	5.70% ←	
238+00	5.70% ←	5.70% ←	
238+50	5.70% ←	5.70% ←	
239+00	5.70% ←	5.70% ←	
239+50	5.70% ←	5.70% ←	
240+00	5.70% ←	5.70% ←	
240+50	5.70% ←	5.70% ←	
241+00	5.70% ←	5.70% ←	
241+50	5.70% ←	5.70% ←	
242+00	5.70% ←	5.70% ←	
242+50	5.70% ←	5.70% ←	
243+00	5.70% ←	5.70% ←	
243+50	5.70% ←	5.70% ←	
243+50.14	5.70% ←	5.70% ←	END FULL SUPER
243+98.80	3.80% ←	3.80% ←	P.T. STATION
244+00	3.75% ←	3.75% ←	
244+44.91	2.00% ←	2.00% ←	RC
244+50	2.00% ←	1.80% ←	
244+96.14	2.00% ←	0.00% →	
245+00	2.00% ←	0.15% →	
245+47.14	2.00% ←	2.00% →	BEGIN NORMAL CROWN - END SUPER

**CTH Y - CURVE 13**

STATION	LEFT	RIGHT	COMMENT
211+57.58	2.00% ←	2.00% →	END NORMAL CROWN - BEGIN SUPER
212+00	0.07% ←	2.00% →	
212+01.58	0.00% →	2.00% →	
212+45.91	2.00% →	2.00% →	RC
212+50	2.18% →	2.18% →	
212+90.24	4.00% →	4.00% →	P.C. STATION
213+00	4.44% →	4.44% →	
213+34.58	6.00% →	6.00% →	BEGIN FULL SUPER
213+50	6.00% →	6.00% →	
214+00	6.00% →	6.00% →	
214+50	6.00% →	6.00% →	
215+00	6.00% →	6.00% →	
215+50	6.00% →	6.00% →	
216+00	6.00% →	6.00% →	
216+50	6.00% →	6.00% →	
217+00	6.00% →	6.00% →	
217+49.43	6.00% →	6.00% →	END FULL SUPER
217+50	5.97% →	5.97% →	
217+93.77	4.00% →	4.00% →	P.T. STATION
218+00	3.72% →	3.72% →	
218+38.10	2.00% →	2.00% →	RC
218+50	1.46% →	1.46% →	
218+82.43	0.00% →	2.00% →	
219+00	0.80% ←	2.00% →	
219+26.43	2.00% ←	2.00% →	BEGIN NORMAL CROWN - END SUPER



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**CTH Y  
STH 153 - CTH II  
MARATHON COUNTY, WISCONSIN**

Date  
**2/28/2013**

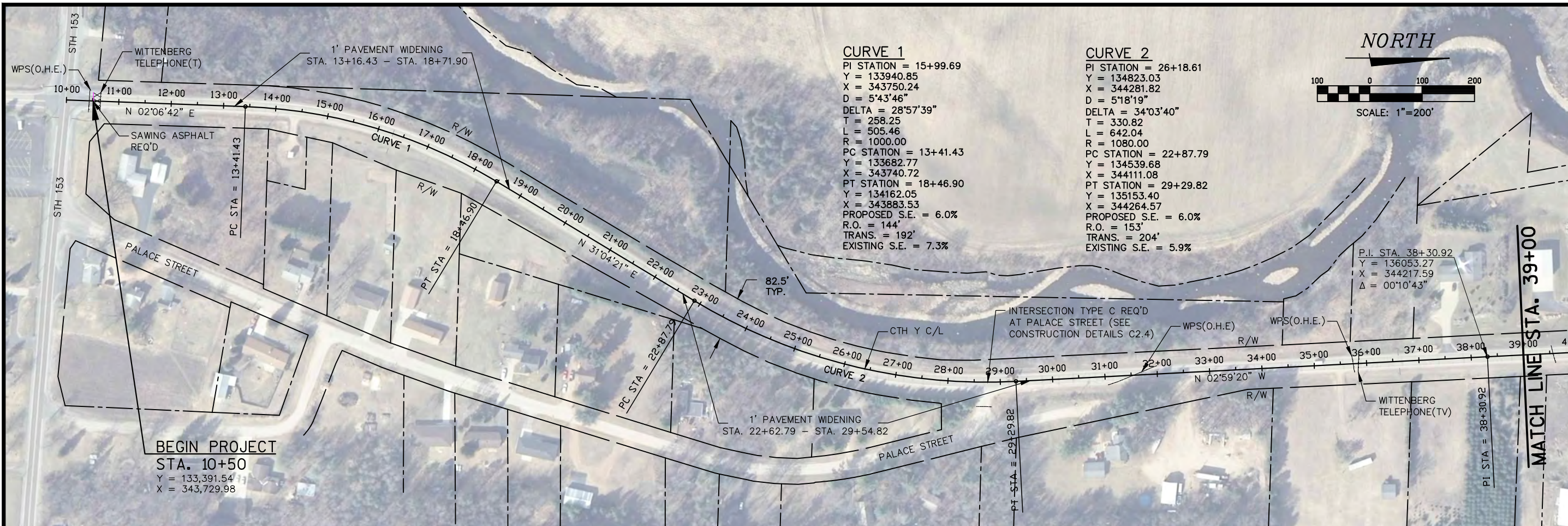
Date	Revision

Drawing Name  
**SUPERELEVATION TABLES**

Sheet No.  
**C2.13**  
Jewell Project Number  
**M97020**

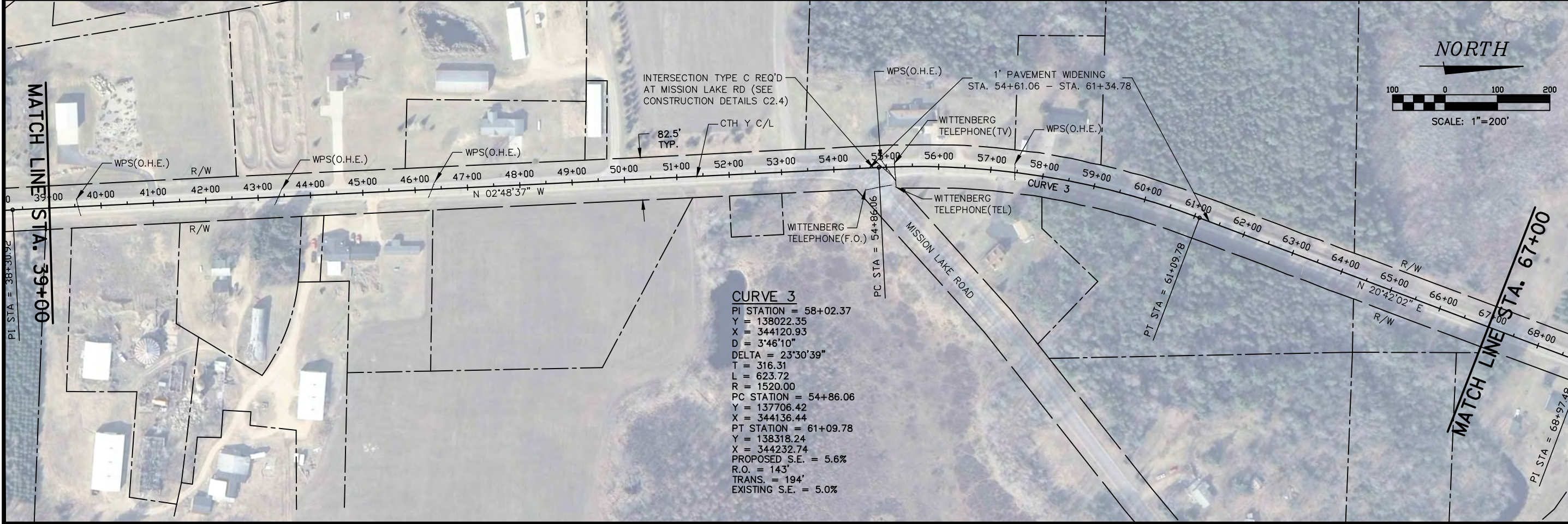
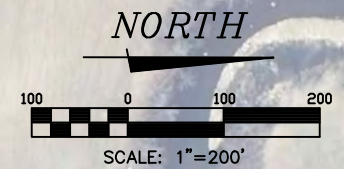




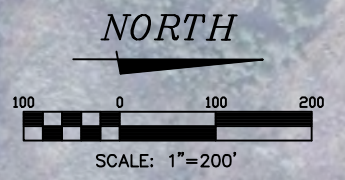


**CURVE 1**  
 PI STATION = 15+99.69  
 Y = 133940.85  
 X = 343750.24  
 D = 5°43'46"  
 DELTA = 28°57'39"  
 T = 258.25  
 L = 505.46  
 R = 1000.00  
 PC STATION = 13+41.43  
 Y = 133682.77  
 X = 343740.72  
 PT STATION = 18+46.90  
 Y = 134162.05  
 X = 343883.53  
 PROPOSED S.E. = 6.0%  
 R.O. = 144'  
 TRANS. = 192'  
 EXISTING S.E. = 7.3%

**CURVE 2**  
 PI STATION = 26+18.61  
 Y = 134823.03  
 X = 344281.82  
 D = 5°18'19"  
 DELTA = 34°03'40"  
 T = 330.82  
 L = 642.04  
 R = 1080.00  
 PC STATION = 22+87.79  
 Y = 134539.68  
 X = 344111.08  
 PT STATION = 29+29.82  
 Y = 135153.40  
 X = 344264.57  
 PROPOSED S.E. = 6.0%  
 R.O. = 153'  
 TRANS. = 204'  
 EXISTING S.E. = 5.9%



**CURVE 3**  
 PI STATION = 58+02.37  
 Y = 138022.35  
 X = 344120.93  
 D = 3°46'10"  
 DELTA = 23°30'39"  
 T = 316.31  
 L = 623.72  
 R = 1520.00  
 PC STATION = 54+86.06  
 Y = 137706.42  
 X = 344136.44  
 PT STATION = 61+09.78  
 Y = 138318.24  
 X = 344232.74  
 PROPOSED S.E. = 5.6%  
 R.O. = 143'  
 TRANS. = 194'  
 EXISTING S.E. = 5.0%



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**CTH Y  
 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**

Date  
**4/9/2013**

Date	Revision

Drawing Name  
**PLAN VIEW**

Sheet No.  
**C2.15**  
 Jewell Project Number  
**M97020**

**CTH Y  
 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**

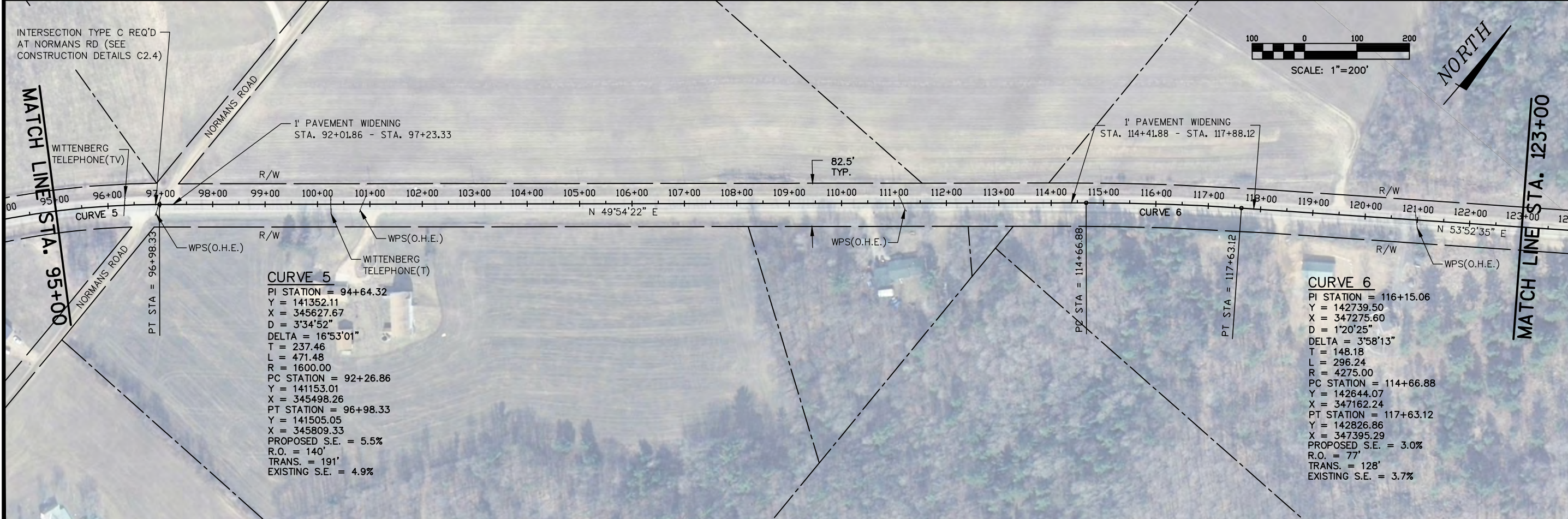
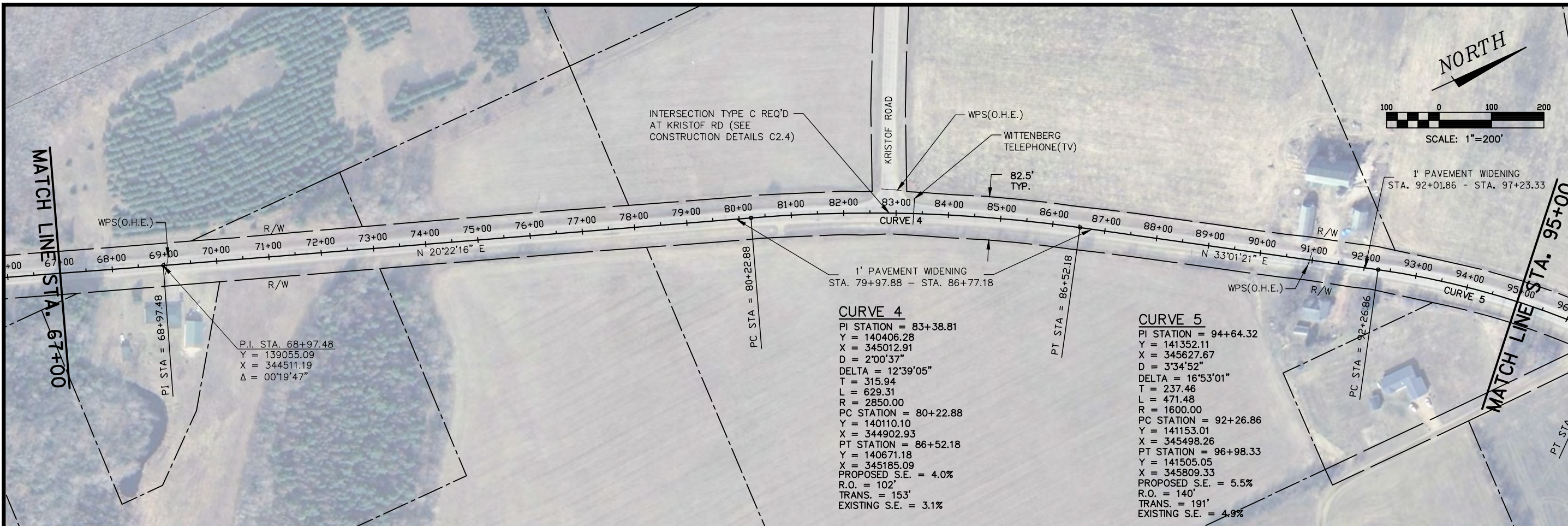
Date  
**4/9/2013**

Date Revision

Drawing Name

**PLAN VIEW**

Sheet No.  
**C2.16**  
 Jewell Project Number  
**M97020**





**CURVE 7**  
 PI STATION = 132+48.31  
 Y = 143702.42  
 X = 348594.95  
 D = 7°38'22"  
 DELTA = 54°48'05"  
 T = 388.78  
 L = 717.35  
 R = 750.00  
 PC STATION = 128+59.53  
 Y = 143473.23  
 X = 348280.92  
 PT STATION = 135+76.88  
 Y = 144091.15  
 X = 348588.67  
 PROPOSED S.E. = 6.0%  
 R.O. = 133'  
 TRANS. = 177'  
 EXISTING S.E. = 6.5%

**CURVE 8**  
 PI STATION = 149+54.20  
 Y = 145468.29  
 X = 348566.44  
 D = 3°49'11"  
 DELTA = 28°29'00"  
 T = 380.72  
 L = 745.69  
 R = 1500.00  
 PC STATION = 145+73.48  
 Y = 145087.62  
 X = 348572.58  
 PT STATION = 153+19.18  
 Y = 145805.81  
 X = 348742.58  
 PROPOSED S.E. = 5.6%  
 R.O. = 143'  
 TRANS. = 194'  
 PROPOSED S.E. = 4.6%

**CURVE 10**  
 PI STATION = 181+88.58  
 Y = 147926.85  
 X = 350510.07  
 D = 3°41'47"  
 DELTA = 15°32'30"  
 T = 211.52  
 L = 420.45  
 R = 1550.00  
 PC STATION = 179+77.06  
 Y = 147838.83  
 X = 350317.73  
 PT STATION = 183+97.50  
 Y = 148063.19  
 X = 350671.79  
 PROPOSED S.E. = 5.5%  
 R.O. = 140'  
 TRANS. = 191'  
 EXISTING S.E. = 6.1%

**CURVE 9**  
 PI STATION = 172+63.65  
 Y = 147529.66  
 X = 349642.19  
 D = 4°52'34"  
 DELTA = 37°51'02"  
 T = 402.87  
 L = 776.22  
 R = 1175.00  
 PC STATION = 168+60.78  
 Y = 147172.50  
 X = 349455.80  
 PT STATION = 176+37  
 Y = 147697.32  
 X = 350008.52  
 PROPOSED S.E. = 6.0%  
 R.O. = 153'  
 TRANS. = 204'  
 EXISTING S.E. = 5.1%

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**CTH Y  
 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**

Date  
**4/9/2013**

Date Revision

Drawing Name  
**PLAN VIEW**

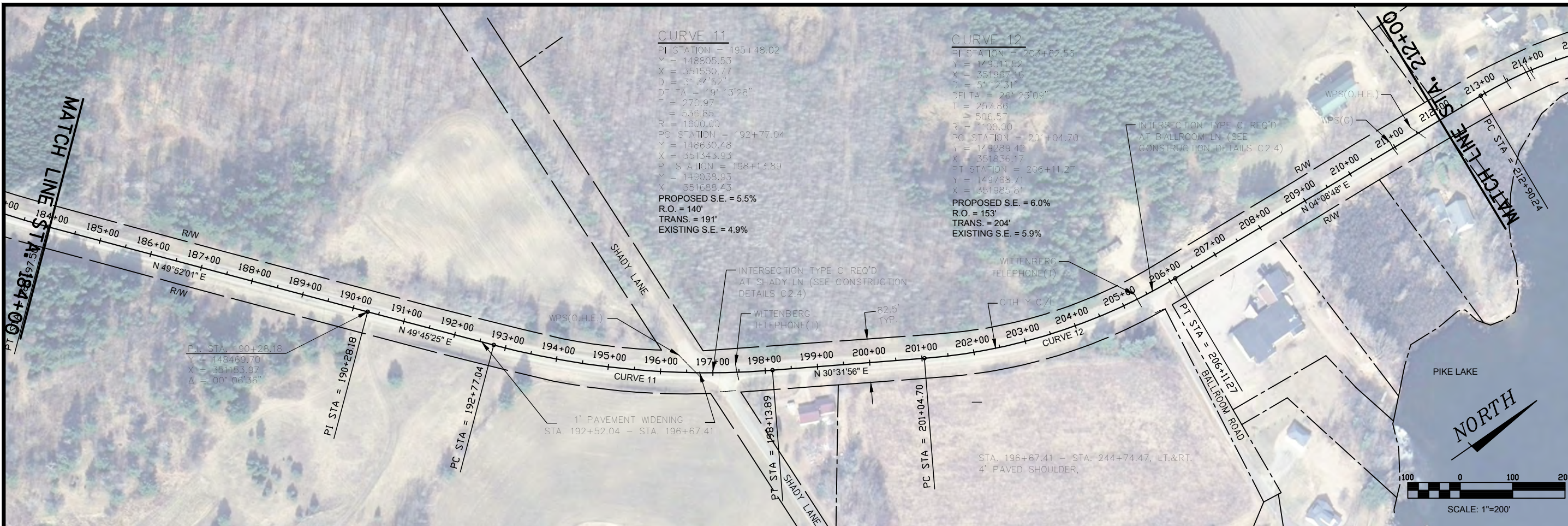
Sheet No.  
**C2.17**  
 Jewell Project Number  
**M97020**

**CTH Y  
 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**

Date  
**4/17/2013**

Date Revision  
 Drawing Name  
**PLAN VIEW**

Sheet No.  
**C2.18**  
 Jewell Project Number  
**M97020**



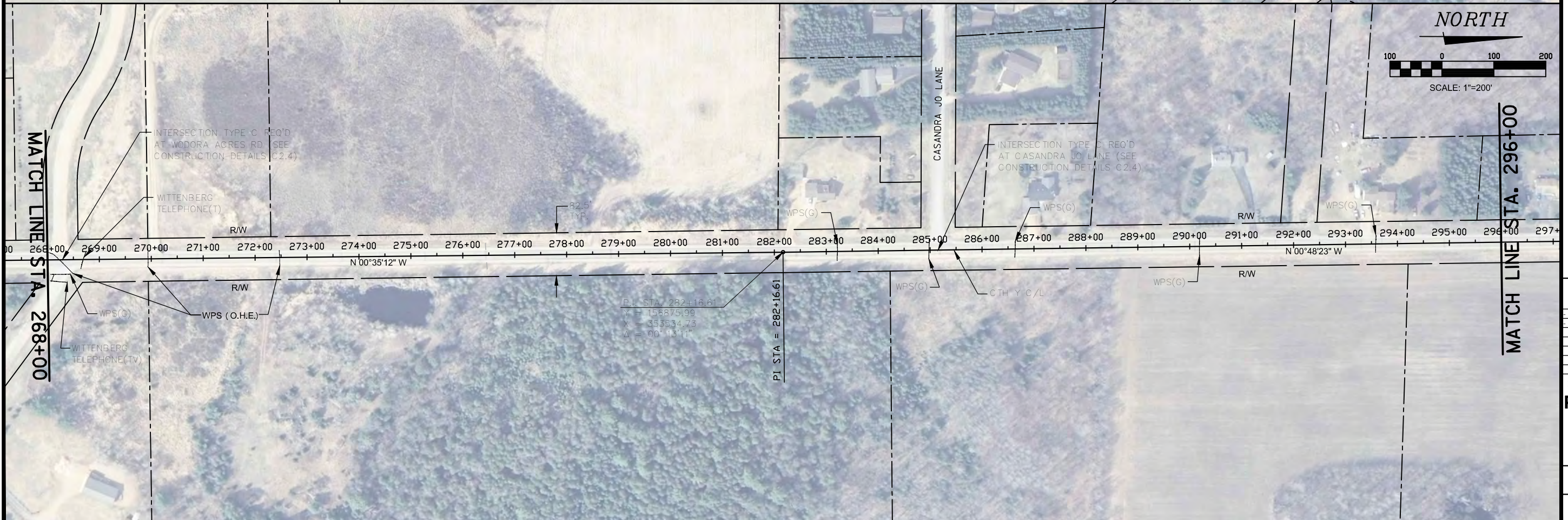
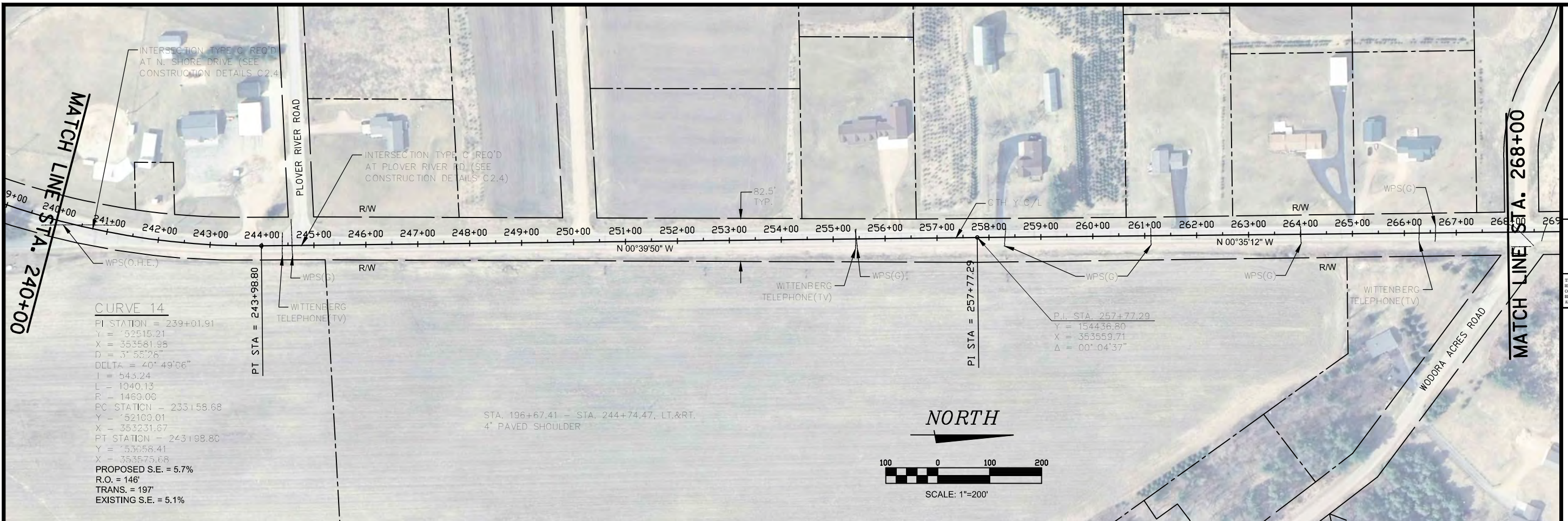
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 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**

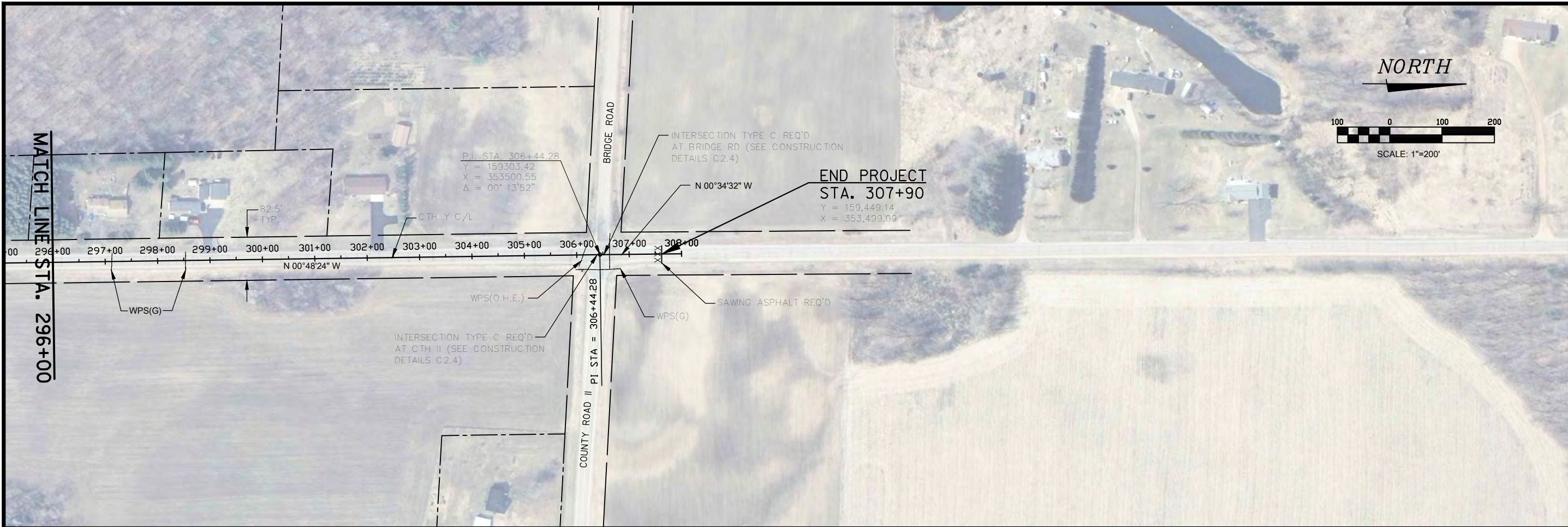
Date  
**4/23/2013**

Date Revision

Drawing Name  
**PLAN VIEW**

Sheet No.  
**C2.19**  
 Jewell Project Number  
**M97020**





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**CTH Y  
 STH 153 - CTH II  
 MARATHON COUNTY, WISCONSIN**

Date  
**4/17/2013**

Date	Revision

Drawing Name  
**PLAN VIEW**

Sheet No.  
**C2.20**  
 Jewell Project Number  
**M97020**

**REMOVING ASPHALTIC SURFACE BUTT JOINTS**

STATION - STATION	LOCATION	204.0115 (SY)
10+50 - 11+25	CTH Y	200
307+15 - 307+90	CTH Y	200
--	PALACE STREET	154
--	MISSION LAKE RD	342
--	KRISTOF RD	296
--	NORMANS RD (SOUTH)	93
--	SHADY LN (EAST)	135
--	SHADY LN (WEST)	201
--	BALLROOM LN	148
--	N. SHORE DR (222+71)	186
--	N. SHORE DR (238+22)	124
--	N. SHORE DR (240+38)	296
--	PLOVER RIVER RD	251
--	WODORA ACRES RD	146
--	CASANDRAM JO LN	135
--	CTH II	378
--	BRIDGE RD	144
--	P.E., F.E., & C.E. (27)	1871
TOTAL =		5300

**REMOVING ASPHALTIC SURFACE MILLING**

STATION - STATION	LOCATION	204.0120 (SY)
11+25 - 307+15	CTH Y	79000
TOTAL =		79000

**FINISHING ROADWAY**

LOCATION	213.0100 (EACH)
PROJECT M97020	1
TOTAL = 1	

**BASE AGGREGATE DENSE**

STATION - STATION	LOCATION	305.0110 3/4-INCH (TON)	305.0120 1 1/4-INCH (TON)
10+50 - 307+90	CTH Y, LT & RT	4586	--
11+97.43 - 19+90.90	CTH Y, CURVE 1	--	308
21+34.79 - 30+82.82	CTH Y, CURVE 2	--	47
53+39.72 - 62+56.12	CTH Y, CURVE 3	--	172
79+03.88 - 87+71.18	CTH Y, CURVE 4	--	285
90+82.52 - 98+42.67	CTH Y, CURVE 5	--	130
113+64.54 - 118+65.46	CTH Y, CURVE 6	--	87
127+26.87 - 137+09.55	CTH Y, CURVE 7	--	174
144+27.15 - 154+65.51	CTH Y, CURVE 8	--	335
167+07.78 - 177+90.00	CTH Y, CURVE 9	--	316
178+32.73 - 185+41.84	CTH Y, CURVE 10	--	114
191+32.71 - 199+58.23	CTH Y, CURVE 11	--	148
199+51.70 - 207+64.28	CTH Y, CURVE 12	--	37
211+57.58 - 219+26.43	CTH Y, CURVE 13	--	40
232+10.34 - 245+47.14	CTH Y, CURVE 14	--	282
--	NORMANS RD (NORTH)	12	--
--	P.E., F.E., C.E. (68)	526	--
--	UNDISTRIBUTED	176	75
TOTALS =		5300	2550

**PULVERIZE AND RELAY**

STATION - STATION	LOCATION	325.0100 (SY)
11+25 - 307+15	CTH Y	79000
TOTAL =		79000

**ASPHALTIC ITEMS**

STATION - STATION	LOCATION	455.0105 ASPHALTIC MATERIAL PG 58-28 (TON)	455.0605 TACK COAT (GAL)	460.1101 HMA PAVEMENT TYPE E-1 (TON)
10+50 - 307+62	CTH Y	922	2082	16755
--	PALACE STREET	2	4	31
--	MISSION LAKE RD	4	9	69
--	KRISTOF RD	3	7	60
--	NORMANS RD (SOUTH)	1	2	19
--	SHADY LN (EAST)	1	3	27
--	SHADY LN (WEST)	2	5	40
--	BALLROOM LN	2	4	30
--	N. SHORE DR (222+71)	2	5	37
--	N. SHORE DR (238+22)	1	3	25
--	N. SHORE DR (240+38)	3	7	60
--	PLOVER RIVER RD	3	6	50
--	WODORA ACRES RD	2	4	29
--	CASANDRAM JO LN	1	3	27
--	COUNTY RD II	4	9	76
--	BRIDGE RD	2	4	29
--	P.E., F.E., & C.E. (27)	17	--	300
--	UNDISTRIBUTED	28	43	536
TOTAL =		1000	2200	18200

NOTE: ASPHALTIC CONTENT IS 5.5%  
HMA PAVEMENT 115 LBS/(SQUARE YARD \* INCH OF THICKNESS)  
TACK COAT 0.025 GAL/SY

**MOBILIZATION**

LOCATION	619.1000 (EACH)
PROJECT M97020	1
TOTAL = 1	

**WATER**

STATION - STATION	LOCATION	624.0100 (MGAL)
10+50 - 307+90	CTH Y, LT & RT	106
--	P.E., F.E., C.E. (68)	8
--	UNDISTRIBUTED	6
TOTALS =		120

**TRAFFIC CONTROL**

LOCATION	643.0100 (EACH)
PROJECT M97020	1
TOTAL = 1	

**PAVEMENT MARKING**

STATION - STATION	LOCATION	DESCRIPTION	646.0103 PAVEMENT MARKING PAINT 4-INCH (LF)	648.0100 LOCATING NO-PASSING ZONES (MI)	649.0100 TEMPORARY PAVEMENT MARKING 4-INCH (LF)
10+50 - 307+90	CTH Y	--	--	5.63	--
10+50 - 307+90	CTH Y, C/L	YELLOW SKIP	--	--	2380
10+50 - 307+90	CTH Y, RT	WHITE EDGE LINE	28860	--	--
10+50 - 307+90	CTH Y, LT	WHITE EDGE LINE	29340	--	--
10+50 - 27+75	CTH Y, C/L	DOUBLE YELLOW	3450	--	--
27+75 - 40+50	CTH Y, C/L	NB PASSING ONLY	1590	--	--
40+50 - 46+50	CTH Y, C/L	PASSING	150	--	--
46+50 - 55+50	CTH Y, C/L	SB PASSING ONLY	1125	--	--
55+50 - 58+60	CTH Y, C/L	DOUBLE YELLOW	620	--	--
58+60 - 69+30	CTH Y, C/L	NB PASSING ONLY	1335	--	--
69+30 - 74+60	CTH Y, C/L	PASSING	130	--	--
74+60 - 83+25	CTH Y, C/L	SB PASSING ONLY	1080	--	--
83+25 - 96+10	CTH Y, C/L	DOUBLE YELLOW	2570	--	--
96+10 - 104+45	CTH Y, C/L	NB PASSING ONLY	1040	--	--
104+45 - 112+60	CTH Y, C/L	PASSING	200	--	--
112+60 - 124+00	CTH Y, C/L	SB PASSING ONLY	1425	--	--
124+00 - 242+00	CTH Y, C/L	DOUBLE YELLOW	23600	--	--
242+00 - 252+25	CTH Y, C/L	NB PASSING ONLY	1280	--	--
252+25 - 280+00	CTH Y, C/L	PASSING	695	--	--
280+00 - 289+60	CTH Y, C/L	SB PASSING ONLY	1200	--	--
289+60 - 300+60	CTH Y, C/L	DOUBLE YELLOW	2200	--	--
300+60 - 307+90	CTH Y, C/L	NB PASSING ONLY	910	--	--
--	CTH II, C/L	DOUBLE YELLOW	100	--	--
--	CTH II, LT & RT	WHITE EDGE LINE	100	--	--
TOTALS =			103000	5.63	2380

**CONSTRUCTION STAKING**

STATION - STATION	LOCATION	650.8000 RESURFACING REFERENCE (LF)	650.9910 SUPPLEMENTAL CONTROL (LS)
10+50 - 307+90	CTH Y	29740	1
TOTALS =		29740	1

**SAWING ASPHALT**

STATION	LOCATION	690.0150 (LF)
10+50	CTH Y	24
30790	CTH Y	24
--	PALACE STREET	24
--	MISSION LAKE RD	22
--	KRISTOF RD	22
--	NORMANS RD (SOUTH)	20
--	SHADY LN (EAST)	22
--	SHADY LN (WEST)	22
--	BALLROOM LN	20
--	N. SHORE DR (222+71)	20
--	N. SHORE DR (238+22)	20
--	N. SHORE DR (240+38)	20
--	PLOVER RIVER RD	22
--	WODORA ACRES RD	22
--	CASANDRAM JO LN	22
--	CTH II	22
--	BRIDGE RD	22
--	P.E., F.E., & C.E. (27)	490
TOTALS =		860



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**CTH Y**  
**STH 153 - CTH II**  
**MARATHON COUNTY, WISCONSIN**

Date  
4/11/2013

Date Revision  
Drawing Name  
**MISCELLANEOUS QUANTITIES**

Sheet No.  
**C3.0**  
Jewell Project Number  
**M97020**