

PLAN OF PROPOSED IMPROVEMENTS CTH Y RESURFACING

STH 153 - CTH II MARATHON COUNTY, WISCONSIN



310 E. Jackson Street Wisconsin Rapids, WI phone: 715-424-2424 fax: 715-424-2421

WISCONSIN

COUNTY

0

E

 α

⋖

S

S

LEGEND & ABBREVIATIONS LEGEND

- SECTION DETAIL

T - TELEPHONE

▲ - SURVEY CONTROL POINT - BUILDING (OBJECT LINE) - FD. LP / REBAR - ROAD/PARKING LOT EDGE - FIRE HYDRANT - FENCE - UTILITY POLE - UTILITY LINE - GUY WIRE - UTILITY LINE - MANHOLE - CONTOURS INTERMEDIATE VALVE (TYPE) - LIGHT POLE - PROPERTY BORDER = - IM FT -LOT LINE 3 - RIGHT OF WAY LINE - PINE TREE

ABBREVIATIONS

O.H.E. - OVERHEAD ELECTRIC

G-GAS F.O. - FIBER OPTIC E-ELECTRIC S.S. - STORM SEWER W.M. - WATER MAIN SAN. - SANITARY SEWER MH - MANHOLE F.M. - FORCE MAIN

PROJECT DIRECTORY

OWNER

MARATHON COUNTY HIGHWAY DEPARTMENT 1430 WEST STREET WAUSAU, WI 54401 PH: (715) 261-1800 ATTN: JIM GRIESBACH

CIVIL ENGINEER

JEWELL ASSOCIATES ENGINEERS, INC. 310 E. JACKSON STREET WISCONSIN RAPIDS, WI 54495 PH: (715)-424-2424 FAX: (715)-424-2421 ATTN: SCOTT WHITSETT EMAIL: SCOTT.WHISETT@JEWELLASSOC.COM

DEPARTMENT OF NATURAL RESOURCES

WISCONSIN RAPIDS SERVICE CENTER 473 GRIFFITH DRIVE WISCONSIN RAPIDS, WI 54494 ATTN: MARC HERSHFIELD PH: (715)-421-7867 EMAIL: MARC.HERSHFIELD@WISCONSIN.GOV



CALL BU 3 WORK DAYS REFORE YOU DIS OR TOLL FREE (800) 242-8511 HEARING IMPARED TTY: (800) 542-2289 www.DiggersHotLine.com

UTILITIES

GAS & ELECTRIC

WISCONSIN PUBLIC SERVICE 700 N. ADAMS STREET GREEN BAY, WI 54301

ATTN: MIKE BOSI - GAS PH: (715) 848-7471

EMAIL: MABOSI@WISCONSINPUBLICSERVICE.COM

ATTN: CLAY VIRCKS - ELECTRIC PH: (715) 848-7317

EMAIL: CHVIRCKS@WISCONSINPUBLICSERVICE.COM

CABLE TV/FIBER OPTIC/TELEPHONE

WITTENBERG TELEPHONE COMPANY 104 W. WALKER STREET PO BOX 160 WITTENBERG, WI 54499

ATTN: DAVE SCHMIDT PH: (715) 881-0308

FRONTIER COMMUNICATIONS 1851 N 14TH AVENUE WAUSAU, WI 54401

ATTN: WARREN INMAN PH: (715) 847-1504

DRAWING INDEX

C1.0 TITLE SHEET TYPICAL SECTIONS C2.0 - C2.3 CONSTRUCTION DETAILS C2.4 - C2.10 TRAFFIC CONTROL PLAN C2.12 - C2.13 SUPERELEVATION TABLES

C2.14 STATION/COORDINATE TABLES C2.15 -C2.20 MISCELLANEOUS QUANTITIES C3.0

DESIGN DATA ORIGINAL PLANS PREPARED BY

DESIGN SPEED 45/50/55 MPH AADT (2013) 1050 AADT (2033) 1600

PROJECT SCOPE

PULVERIZE AND RELAY, HMA PAVEMENT TYPE E-1, AND BASE AGGREGATE SHOULDERS

5.633 MILES



02-21-2013

GENERAL NOTES

COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS).

THE EXACT LOCATION AND WIDTH OF PRIVATE, COMMERCIAL, AND FILED ENTRANCES TO BE DETERMINED BY THE ENGINEER IN THE FIELD. EXISTING ENTRANCES WILL BE RESTORED IN KIND AS DIRECTED BY THE ENGINEER IN THE FIELD.

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA WHICH ARE NOT SHOWN ON THE PLANS, THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND OR A DIRECT CALL TO THE UTILITIES WHICH HAVE FACILITIES IN THE AREA.

ACCESS TO ALL ENTRANCES SHALL BE MAINTAINED DURING CONSTRUCTION.

3.5 INCHES OF HMA PAVEMENT TYPE E-1 SHALL BE CONSTRUCTED WITH A LOWER LAYER OF 1.75 INCHES AND AN UPPER LAYER OF 1.75 INCHES. THE NOMINAL SIZE OF AGGREGATE USED SHALL BE 12.5 MM.

REMOVAL OF ASPHALTIC SURFACES WHERE AN ABUTTING ASPHALTIC SURFACE IS TO REMAIN IN PLACE SHALL REQUIRE A SAWCUT MEETING THE APPROVAL OF THE ENGINEER IN THE FIELD.

HMA PAVEMENT TYPE E-1 QUANTITIES WERE CALCULATED BASED ON DENSITY OF 115 LB/SY-IN.

TACK COAT HAS BEEN ESTIMATED AT AN APPLICATION RATE OF 0.025 GAL/SY.

PAVING LIMITS AT INTERSECTIONS AND ENTRANCES SHALL MATCH THE EXISTING RADII AND TAPER LENGTHS.

DESIGN IS IN ACCORDANCE WITH "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS" 2004 (AASHTO) EXCEPT AS PROVIDED FOR IN THE SPECIAL PROVISIONS, ALL WORK SHALL BE ACCOMPLISHED ACCOMPLISHED FOR THE STATE OF WISCONSIN "STANDARD SPECIFICATIONS". FOR HIGHWAY AND STRUCTURE CONSTRUCTION" 2013 FOITION.

MARATHON COUNTY

RECOMMENDED FOR APPROVAL

trawing Name

TITLE SHEET

C1.0



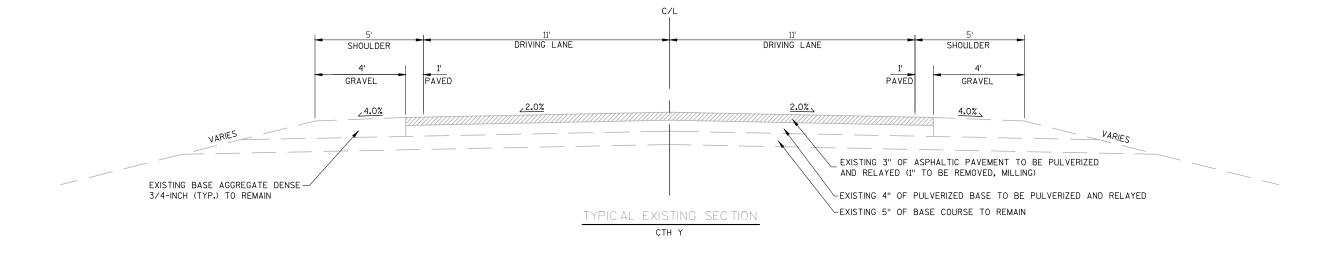
560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

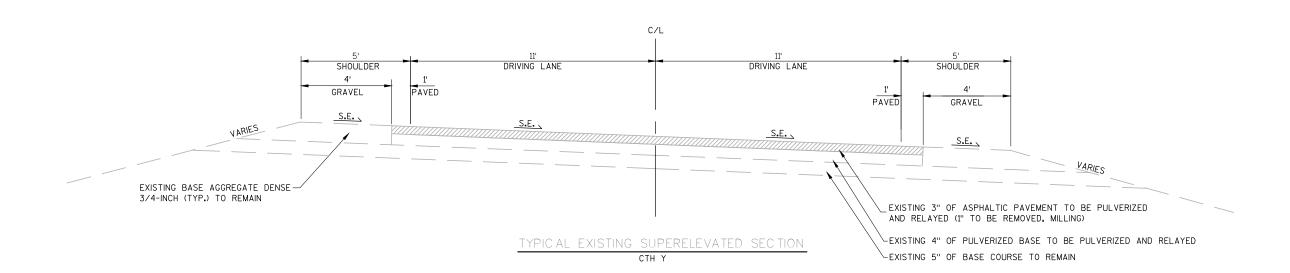
MARATHON COUNTY, WISCONSIN 153 - CTH II CTH STH

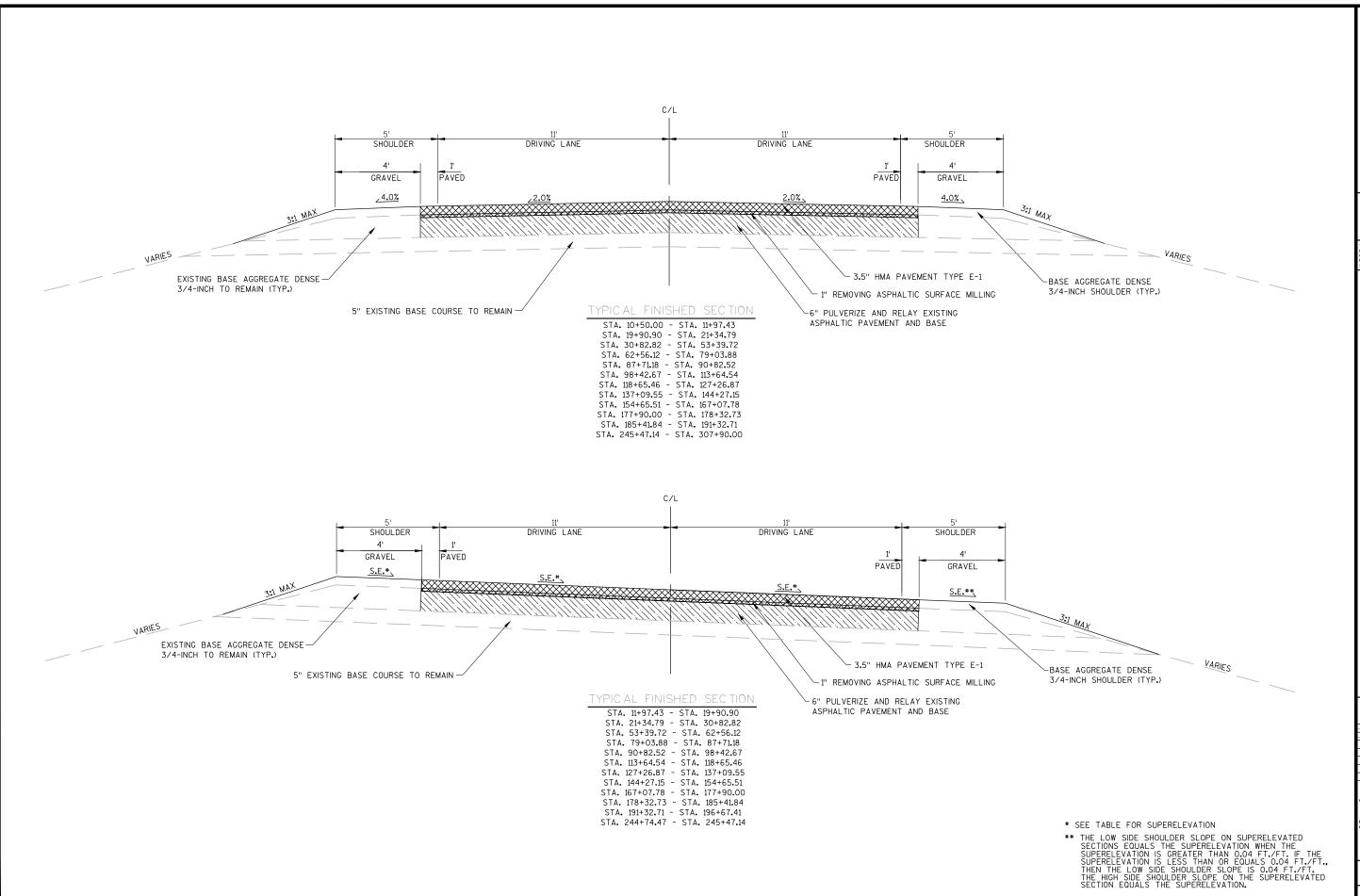
4/17/2013 Drawing Name

TYPICAL SECTIONS

> C2.0 M97020







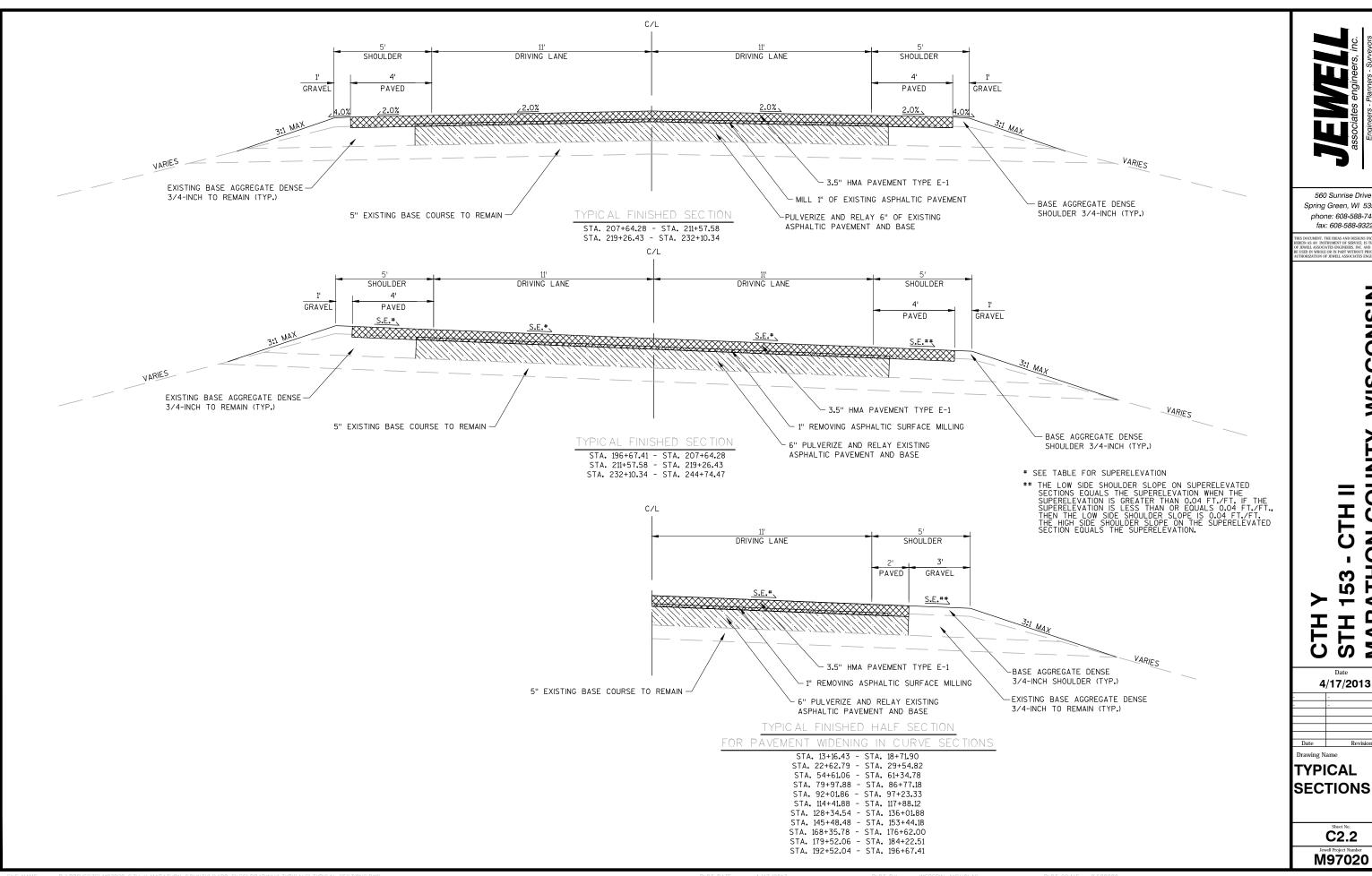
560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

WISCONSIN COUNTY, CTH MARATHON 53 STH C

4/17/2013 Date Drawing Name

TYPICAL SECTIONS

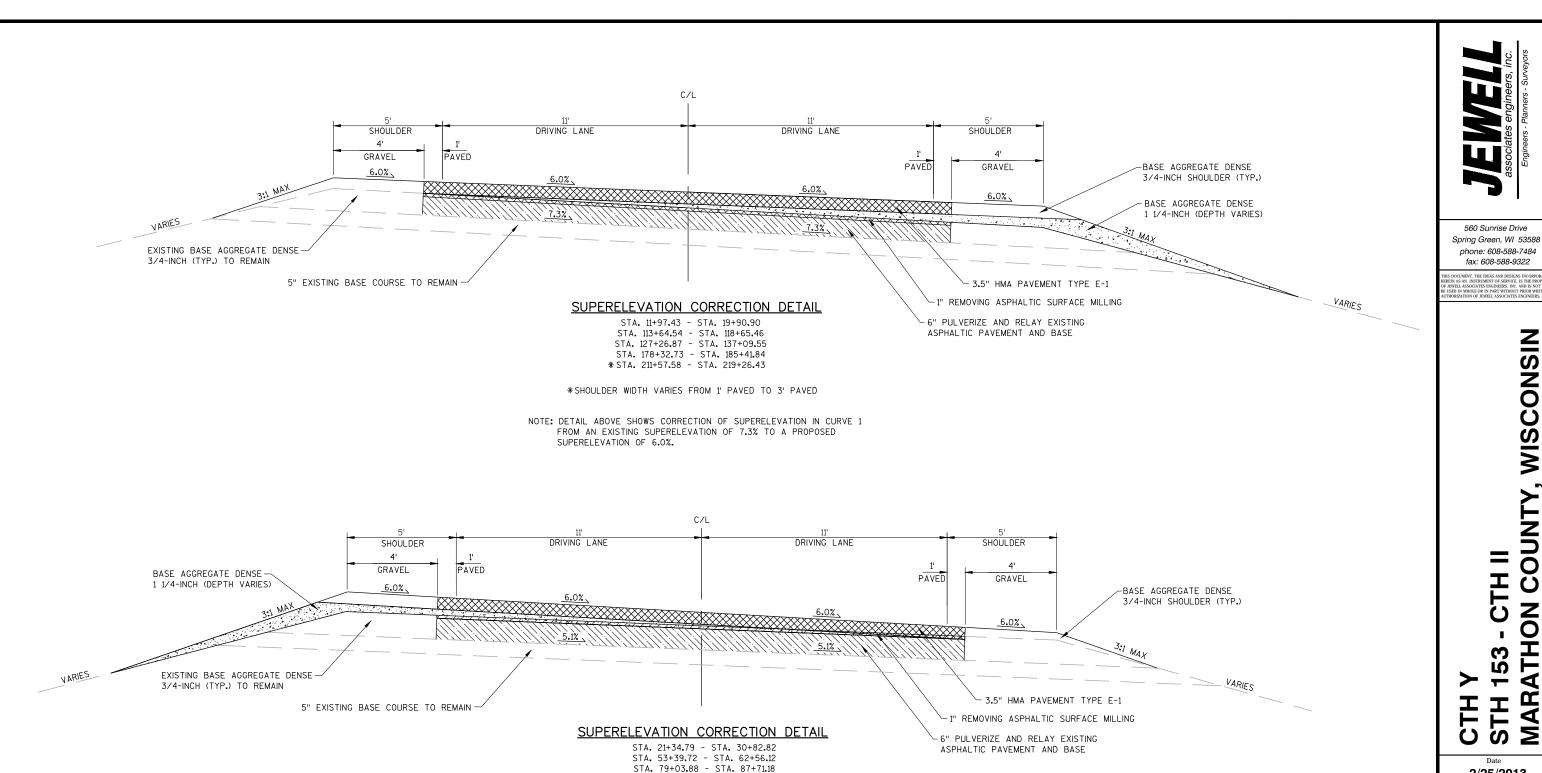
C2.1



Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

WISCONSIN COUNTY, NO O

4/17/2013



STA. 90+82.52 - STA. 98+42.67 STA. 144+27.15 - STA. 154+65.51 STA. 167+07.78 - STA. 177+90.00 *****STA. 191+32.71 - STA. 199+58.23 *STA. 199+51.70 - STA. 207+64.28 *STA. 232+10.34 - STA. 245+47.14 *SHOULDER WIDTH VARIES FROM 1' PAVED TO 3' PAVED

NOTE: DETAIL ABOVE SHOWS CORRECTION OF SUPERELEVATION IN CURVE 9

FROM AN EXISTING SUPERELEVATION OF 5.1% TO A PROPOSED

SUPERELEVATION OF 6.0%.

WISCONSIN COUNTY, CTH NO O 53 STH

2/25/2013

Drawing Name

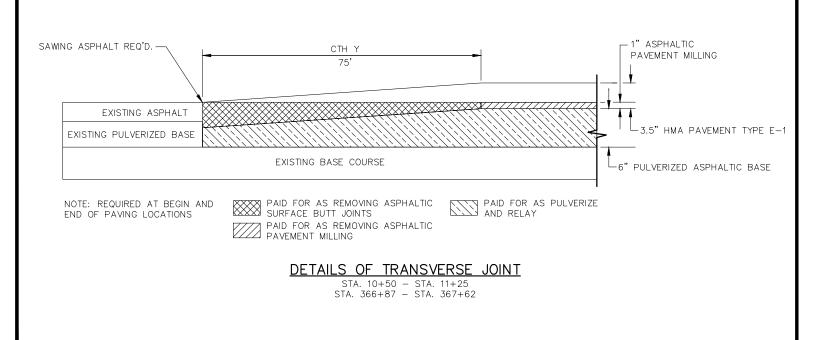
TYPICAL SECTIONS SUPERELEVATION CORRECTION

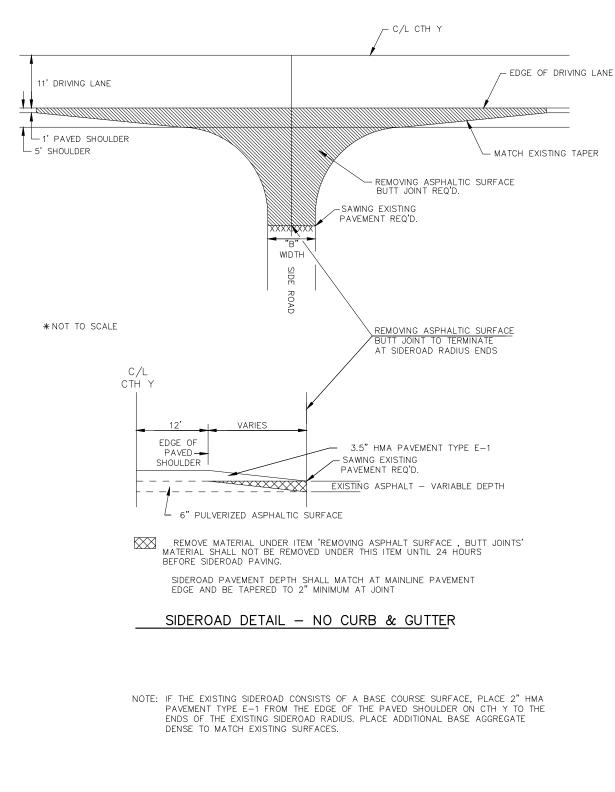
M97020

C2.3

SIDEROAD DATA TABLE

			MATCH	"B"
SIDEROAD	LOCATION	TYPE	EX. AT	WIDTH
OLD COUNTY RD Y/PALACE STREET	MAINLINE, RT	ASPHALT	40' RT	24'
MISSION LAKE ROAD	MAINLINE, RT	ASPHALT	60' RT	22'
KRISTOF ROAD	MAINLINE, LT	ASPHALT	60' LT	22'
NORMANS ROAD (SOUTH)	MAINLINE, RT	ASPHALT	35' RT	20'
NORMANS ROAD (NORTH)	MAINLINE, LT	B.A.D.	35' LT	20'
SHADY LANE (EAST)	MAINLINE, RT	ASPHALT	40' RT	22'
SHADY LANE (WEST)	MAINLINE, LT	ASPHALT	50' LT	22'
BALLROOM LANE	MAINLINE, RT	ASPHALT	40' RT	20'
N. SHORE DRIVE (STA. 222+70.50)	MAINLINE, RT	ASPHALT	45' RT	20'
N. SHORE DRIVE (STA. 240+38.42)	MAINLINE, RT	ASPHALT	50' RT	20'
PLOVER RIVER ROAD	MAINLINE, LT	ASPHALT	60' LT	22'
WODORA ACRES ROAD	MAINLINE, RT	ASPHALT	45' RT	22'
CASANDRAM JO LANE	MAINLINE, LT	ASPHALT	45' LT	22'
COUNTY ROAD II	MAINLINE, RT	ASPHALT	90' RT	22'
BRIDGE ROAD	MAINLINE, LT	ASPHALT	40' LT	22'





560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

DOCUMENT, THE IDEAS AND DESIGNS INCORPORATE IN AS AN INSTRUMENT OF SERVICE, IS THE PROPEE REVIEL ASSOCIATES ENGINEERS, INC. AND IS NOT TO. SED IN WHOLE OR IN PART WITHOUT PRIOR WRITTH HORIZATION OF JEWELL ASSOCIATES ENGINEERS, IN

COUNTY, WISCONSIN ATHON Ŋ STH $\mathbf{\alpha}$ ₹

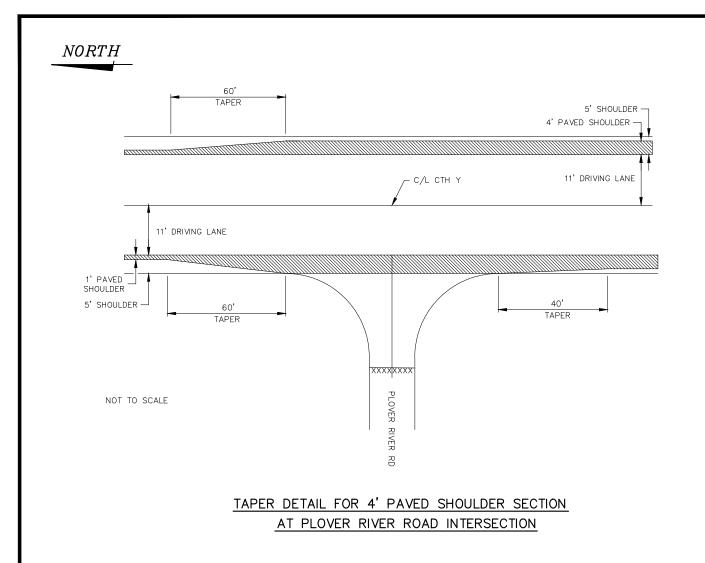
4/18/2013

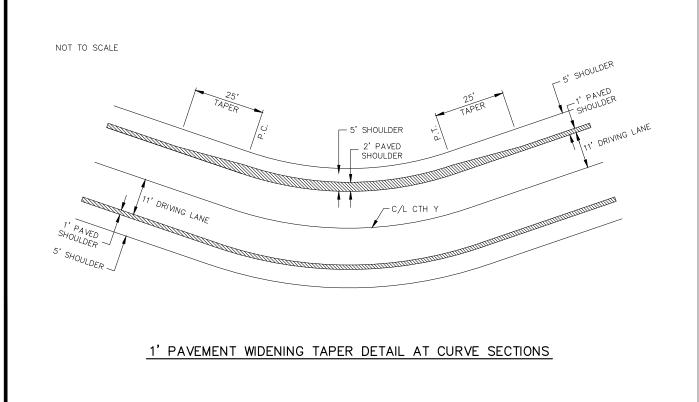
Date

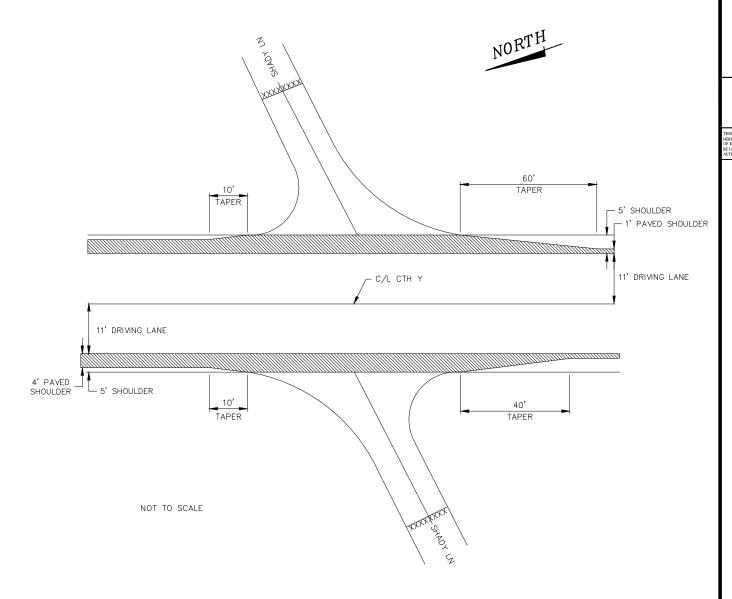
Ċ

Drawing Name CONSTRUCTION **DETAILS**

C2.4







TAPER DETAIL FOR 4' PAVED SHOULDER SECTION AT SHADY LANE INTERSECTION



153 CTH Y STH

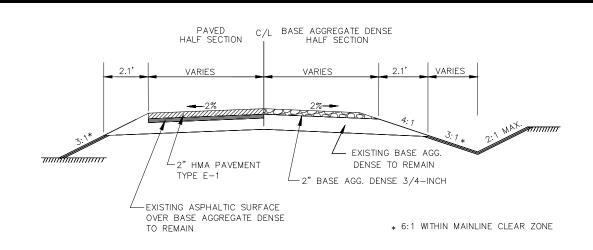
2/28/2013

Drawing Name CONSTRUCTION **DETAILS**

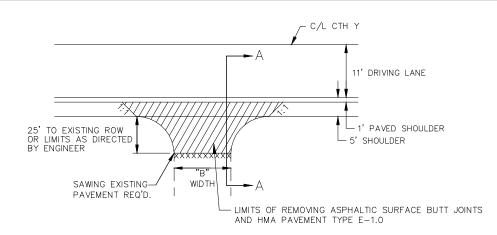
C2.5

DRIVEWAY INFORMATION

			MATCH	"B"				MATCH	"B"
STATION	LOCATION	TYPE	EX. AT	WIDTH	STATION	LOCATION	TYPE	EX. AT	WIDTH
10+84.20	MAINLINE, RT	B.A.D.	41' RT	12'	169+36.74	MAINLINE, LT	B.A.D.	41' LT	15'
14+40.11	MAINLINE, RT	B.A.D.	41' RT	12'	169+69.22	MAINLINE, RT	B.A.D.	41' RT	14'
22+61.60	MAINLINE, LT	B.A.D.	41' LT	15'	171+39.66	MAINLINE, LT	B.A.D.	41' LT	13'
32+46.91	MAINLINE, RT	B.A.D.	41' RT	29,	177+45.71	MAINLINE, RT	B.A.D.	41' RT	14'
35+50.00	MAINLINE, RT	B.A.D.	41' RT	13'	177+51.16	MAINLINE, LT	B.A.D.	41' LT	14'
35+80.05	MAINLINE, LT	B.A.D.	41' LT	16'	190+31.93	MAINLINE, RT	B.A.D.	41' RT	14'
37+55.85	MAINLINE, LT	ASPHALT	41' LT	14'	198+72.14	MAINLINE, RT	B.A.D.	41' RT	13'
40+96.66	MAINLINE, LT	ASPHALT	41' LT	15'	205+43.72	MAINLINE, LT	B.A.D.	41' LT	15'
42+81.16	MAINLINE, RT	B.A.D.	41' RT	18'	207+65.65	MAINLINE, RT	ASPHALT	41' RT	77'
43+88.69	MAINLINE, RT	B.A.D.	41' RT	20,	208+89.92	MAINLINE, LT	B.A.D.	41' LT	11'
44+55.52	MAINLINE, LT	B.A.D.	41' LT	18'	211+47.50	MAINLINE, RT	B.A.D.	41' RT	12'
45+14.50	MAINLINE, RT	B.A.D.	41' RT	20'	211+76.67	MAINLINE, LT	B.A.D.	41' LT	35,
47+91.40	MAINLINE, LT	ASPHALT	41' LT	17'	216+65.28	MAINLINE, LT	ASPHALT	41' LT	14'
49+84.03	MAINLINE, LT	ASPHALT	41' LT	12'	219+08.55	MAINLINE, RT	ASPHALT	41' RT	11'
51+22.87	MAINLINE, RT	B.A.D.	41' RT	16'	219+32.88	MAINLINE, RT	ASPHALT	41'RT	11'
51+90.00	MAINLINE, RT	B.A.D.	41' RT	16'	220+13.32	MAINLINE, RT	ASPHALT	41' RT	32'
53+14.24	MAINLINE, LT	ASPHALT	41' LT	14'	221+25.32	MAINLINE, RT	ASPHALT	41' RT	18'
55+75.20	MAINLINE, LT	ASPHALT	41'LT	20'	221+32.14	MAINLINE, LT	ASPHALT	41' LT	22'
56+37.62	MAINLINE, LT	B.A.D.	41'LT	14'	223+66.83	MAINLINE, LT	ASPHALT	41' LT	13'
57+22.28	MAINLINE, RT	ASPHALT	41' RT	12'	228+27.82	MAINLINE, LT	B.A.D.	41' LT	20'
57+96.63	MAINLINE, LT	ASPHALT	41' LT	22'	228+27.82	MAINLINE, RT	B.A.D.	41' RT	19'
64+44.91	MAINLINE, RT	B.A.D.	41' RT	22.	231+25.55	MAINLINE, RT	B.A.D.	41' RT	16'
67+89.68	MAINLINE, LT	B.A.D.	41' LT	15'	234+66.40	MAINLINE, RT	B.A.D.	41' RT	13'
69+15.05	MAINLINE, RT	ASPHALT	41' RT	20'	237+88.37	MAINLINE, LT	B.A.D.	41' LT	12'
80+77.03	MAINLINE, RT	B.A.D.	41' RT	14'	238+20.64	MAINLINE, RT	ASPHALT	41' RT	18'
91+34.09	MAINLINE, LT	B.A.D.	41'LT	41'	241+89.72	MAINLINE, LT	B.A.D.	41' LT	17'
91+66.22	MAINLINE, RT	B.A.D.	41' RT	13'	243+72.62	MAINLINE, LT	B.A.D.	41' LT	22'
100+99.80	MAINLINE, RT	B.A.D.	41' RT	15'	248+24.05	MAINLINE, RT	B.A.D.	41' RT	28'
101+24.11	MAINLINE, LT	B.A.D.	41'LT	13'	248+54.43	MAINLINE, LT	B.A.D.	41' LT	32'
110+52.62	MAINLINE, RT	B.A.D.	41' RT	16'	256+11.96	MAINLINE, LT	B.A.D.	41' LT	16'
111+55.89	MAINLINE, LT	B.A.D.	41' LT	14'	258+18.26	MAINLINE, LT	ASPHALT	41' LT	14'
118+39.21	MAINLINE, RT	B.A.D.	41' RT	15'	260+31.18	MAINLINE, RT	B.A.D.	41' RT	30,
119+49.50	MAINLINE, LT	B.A.D.	41' LT	20'	261+70.87	MAINLINE, LT	ASPHALT	41' LT	18'
120+76.95	MAINLINE, RT	ASPHALT	41' RT	13'	265+06.40	MAINLINE, LT	ASPHALT	41' LT	23'
124+77.08	MAINLINE, RT	B.A.D.	41' RT	14'	270+07.00	MAINLINE, LT	B.A.D.	41' LT	15'
128+10.43	MAINLINE, RT	ASPHALT	41' RT	12'	272+16.10	MAINLINE, RT	B.A.D.	41' RT	19'
130+69.30	MAINLINE, RT	B.A.D.	41' RT	19'	278+58.43	MAINLINE, LT	B.A.D.	41' LT	17'
131+33.70	MAINLINE, RT	B.A.D.	41' RT	18'	282+31.90	MAINLINE, LT	B.A.D.	41' LT	14'
137+01.80	MAINLINE, RT	B.A.D.	41' RT	20'	286+98.13	MAINLINE, LT	B.A.D.	41' LT	41'
139+26.24	MAINLINE, LT	B.A.D.	41' LT	15'	291+23.42	MAINLINE, LT	ASPHALT	41' LT	12'
139+51.50	MAINLINE, RT	B.A.D.	41' RT	11'	291+64.58	MAINLINE, RT	B.A.D.	41' RT	19'
140+72.90	MAINLINE, LT	B.A.D.	41' LT	12'	292+39.87	MAINLINE, LT	B.A.D.	41' LT	10'
144+34.97	MAINLINE, RT	B.A.D.	41' RT	18'	293+40.28	MAINLINE, LT	B.A.D.	41' LT	24'
145+68.44	MAINLINE, RT	B.A.D.	41' RT	13'	296+83.02	MAINLINE, LT	ASPHALT	41' L T	18'
145+71.75	MAINLINE, LT	B.A.D.	41' LT	16'	298+97.34	MAINLINE, LT	ASPHALT	41' LT	17'
148+10.19	MAINLINE, LT	B.A.D.	41' LT	14'	302+20.57	MAINLINE, LT	ASPHALT	41' LT	55,
150+56.22	MAINLINE, LT	B.A.D.	41' LT	18'	304+72.79	MAINLINE, LT	B.A.D.	41' LT	16'
152+25.07	MAINLINE, LT	B.A.D.	41' LT	16'					



TYPICAL SECTION FOR PRIVATE ENTRANCES



ANY ADDITIONAL BASE AGG. DENSE REQ'D. SHALL BE PAID UNDER ITEM — "BASE AGGREGATE DENSE 3/4-INCH"

RURAL DRIVEWAY DETAIL - ASPHALT

CTH Y
C/L

11'
5'
ROW OR

DRIVING LANE
PAVED
LIMITS AS
DIRECTED BY
ENGINEER

2" REMOVING ASPHALTIC
SURFACE BUTT JOINTS

3.5" PAVEMENT TYPE E-1

6" PULVERIZED ASPHALTIC SURFACE
EXISTING ASPHALTIC
SURFACE TO REMAIN

SECTION A-A

associates engineers, inc.

560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

OCUMENT, THE IDEAS AND DESIGNS INCORPORATED N AS AN INSTRUMENT OF SERVICE, IS THE PROPERTY VELL ASSOCIATES ENGINEERS, INC. AND IS NOT TO DIN WHOLE OR IN PART WITHOUT PRIOR WRITTEN DRIVATION OF IEWEL ASSOCIATES FROGINEERS INC

STH 153 - CTH II MARATHON COUNTY, WISCONSIN

Date
4/18/2013

Date Revision

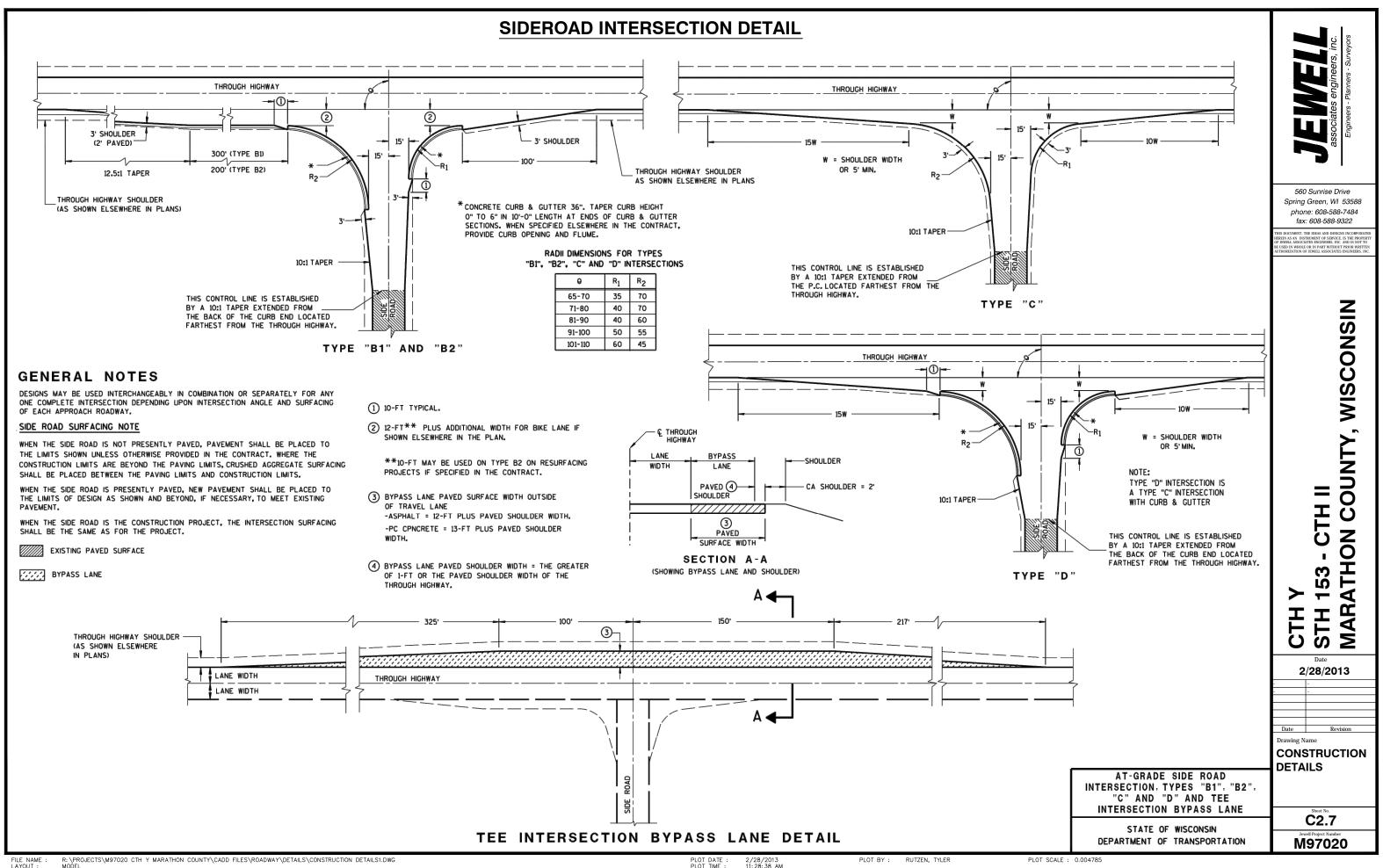
Drawing Name

CONSTRUCTION

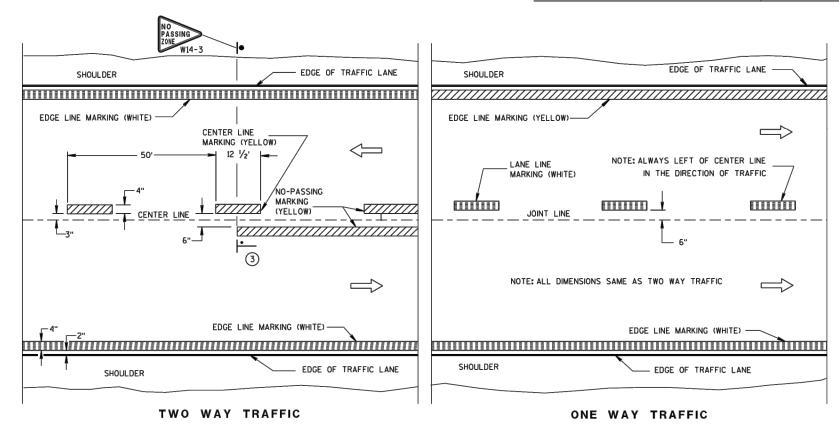
DETAILS

C2.6

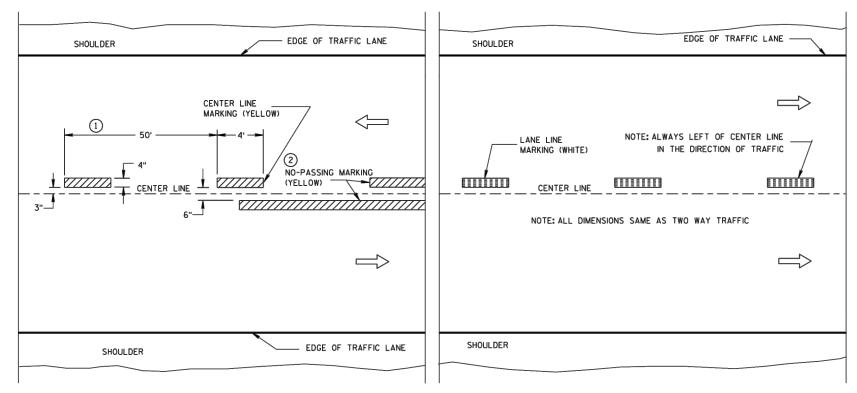
Jewell Project Numbe



PAVEMENT MARKING (MAINLINE) DETAIL



PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING

(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

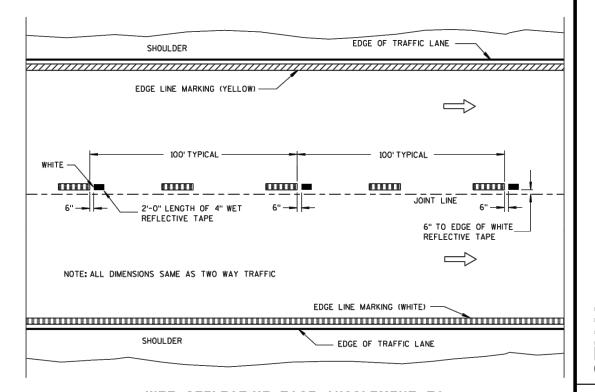
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- (1) HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- (2) NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- (3) NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.

NOTE

ARROW SYMBOL () SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE



RUTZEN, TYLER

PAVEMENT MARKING (MAINLINE)

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION APPROVED

/S/ Thomas N. Notbohm 6-23-11 STATE TRAFFIC ENGINEER OF DESIG

560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

IN WHOLE OR IN PART WITHOUT PRIOR W

COUNTY, WISCONSIN ATHON 3 MAR/

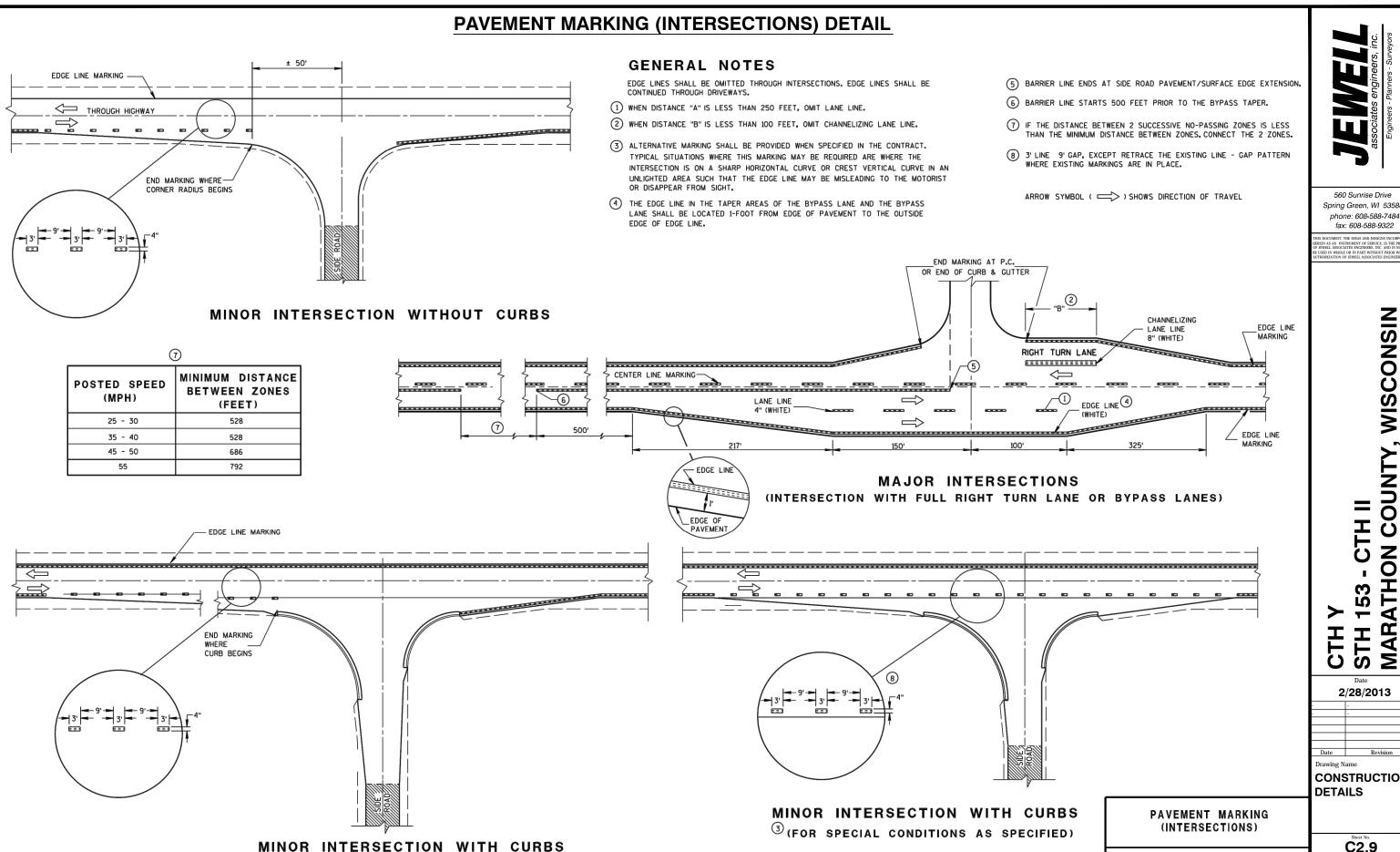
CONSTRUCTION **DETAILS**

ST

2/28/2013

C2.8 M97020

DATE



STATE OF WISCONSIN

DEPARTMENT OF TRANSPORTATION

560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484

COUNTY, WISCONSIN

2/28/2013

CONSTRUCTION

M97020

PLOT BY: RUTZEN, TYLER

(TYPICAL MARKING)

TRAFFIC CONTROL MOVING OPERATIONS DETAIL

TWO-LANE ROADWAY



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7g AND W20-4 SIGNS, A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.

ROAD

AHEAD

W20-

Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322 2

ROAD

WORK

AHEAD

W20-

S DOCUMENT, THE IDEAS AND DESIGNS INCORPORAT EIN AS AN INSTRUMENT OF SERVICE, IS THE PROPEE JEWELL ASSOCIATES ENGINEERS, INC. AND IS NOT TO ISED IN WHOLE OR IN PART WITHOUT PRIOR WRITTH HORIZATION OF JEWELL ASSOCIATES ENGINEERS, IN

560 Sunrise Drive

WISCONSIN COUNTY ATHON 4

2/28/2013

Prawing Name

CONSTRUCTION **DETAILS**

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL FOR LANE

CLOSURE (SUITABLE FOR

MOVING OPERATIONS)

/S/ Thomas N. Notbohm STATE TRAFFIC ENGINEER OF DESIGN

SYMBOLS WORK AREA FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF SIGN ON PORTABLE OR PERMANENT SUPPORT

- 500'(TYPICAL) -- 200' TO 300' (TYPICAL) --500' MIN - 500' (TYPICAL) -VARIABLE DISTANCE 48" X 24" END ROAD WORK G20-2 WORK AREA LIMITS 48" X 24" ROAD WORK G20-2 VARIABLE DISTANCE -

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, THE "FLAGGER AHEAD", THE "ROAD WORK AHEAD" AND THE ONE LANE ROAD AHEAD" SIGNS SHALL BE COVERED OR REMOVED AND THE HIGHWAY RESTORED TO NORMAL OPERATION.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION

- 500' (TYPICAL)

ROAD

AHEAD

500' MIN.

1

- 200' TO 300' (TYPICAL) -

OR AS APPROVED BY THE ENGINEER.

SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

R:\PROJECTS\M97020 CTH Y MARATHON COUNTY\CADD FILES\ROADWAY\DETAILS\CONSTRUCTION DETAILS1.DWG MODEL

500' (TYPICAL)

ROAD

WORK

AHEAD

PLOT BY: RUTZEN, TYLER

PLOT SCALE: 0.004785

C2.10











GENERAL NOTES

THE EXACT LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPRORIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

ALL SIGNS SHALL BE 48" x 48" UNLESS OTHERWISE NOTED.

ALL ROADS AND STREETS WITHIN THE WORK ZONES SHALL BE KEPT ACCESSIBLE FOR EMERGENCY VEHICLES, RESIDENTS AND BUSINESSES.

ANY STOP SIGNS WHICH ARE REMOVED FOR A CONSTRUCTION OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED.

ALL SIGN LAYOUT SHALL BE IN ACCORDANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL OF STANDARD HIGHWAY SIGNS.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AND MAINTAIN ACCESS TO ALL PROPERTY ABUTTING THE ROADWAY CONSTRUCTION WORK THROUGHOUT THE LIFE OF THE PROJECT.

THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL NECESSARY BARRICADES, SIGNS, LIGHTS, TEMPORARY MARKINGS, FLAGGERS, AND SUCH OTHER SAFETY DEVICES AS CALLED FOR ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

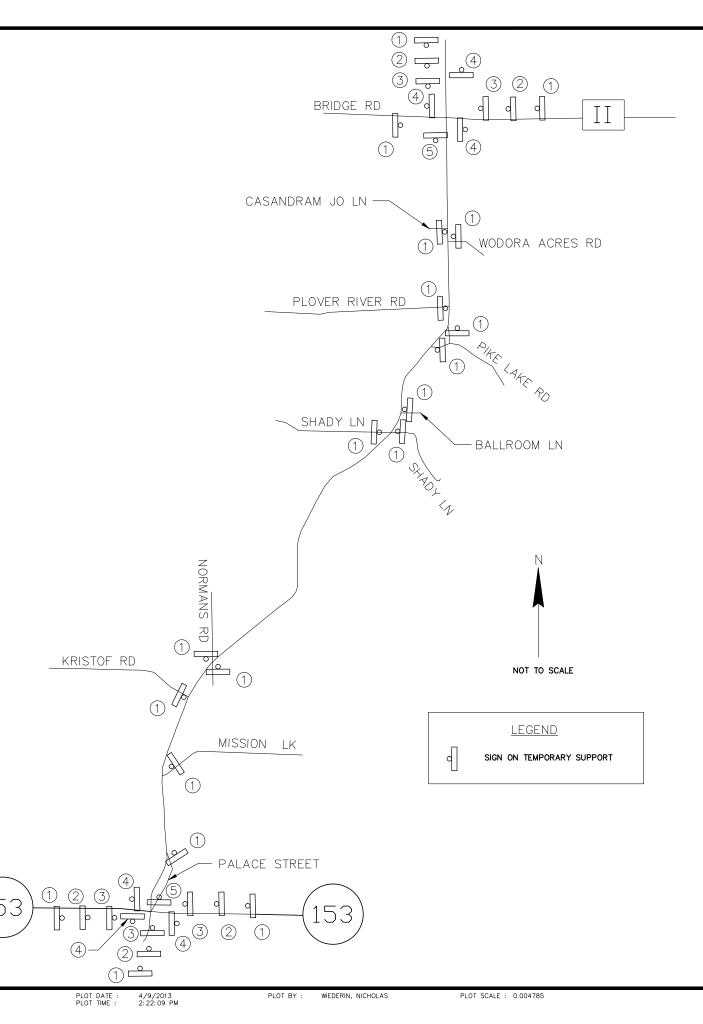
THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200' CLEARANCE TO EXISTING SIGNS.

DURING NIGHT SHUTDOWN, ONE LANE IN EACH DIRECTION MUST REMAIN OPEN. (ON A HARD SURFACE FOR MAINLINE, OR BASE AGGREGATE DENSE FOR SIDE ROADS).

DRAWINGS SHOW TRAFFIC CONTROL FOR A TYPICAL SITUATION. ADDITIONAL TRAFFIC CONTROL DEVICES MAY BE REQUIRED AND/OR LAYOUT DETAILS MODIFIED DEPENDING ON CONTRACTOR'S METHODS OR SEQUENCES OF OPERATION.

ROAD MACHINERY, FLAGGERS AHEAD, ETC. SIGNS SHALL BE USED AS NEEDED AND SHALL BE REMOVED OR COVERED AT NIGHTS, ON WEEKENDS OR WHEN THE ACTIVITY DOES NOT EXIST.

SIGN PLACEMENT SHALL FOLLOW SDD, "TRAFFIC CONTROL ADVANCE WARNINGS SIGNS 45 M.P.H. OR GREATER TWO—WAY UNDIVIDED ROAD OPEN TO TRAFFIC" UNLESS OTHERWISE NOTED IN THE TRAFFIC CONTROL PLAN.



560 Sunrise Drive

Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

S DOCUMENT, THE IDEAS AND DESIGNS INCORPORAT EIN AS AN INSTRUMENT OF SERVICE, IS THE PROPEE JEWELL ASSOCIATES ENGINEERS, INC. AND IS NOT TO ISED IN WHOLE OR IN PART WITHOUT PRIOR WRITTH HORIZATION OF JEWELL ASSOCIATES ENGINEERS, IN

WISCONSIN

COUNTY

O

 $\mathbf{\alpha}$

⋖

Ž

ST

4/9/2013

TRAFFIC CONTROL

C2.11

M97020

PLAN

560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

MARATHON COUNTY, WISCONSIN 153

2/28/2013

Drawing Name SUPERELEVATION **TABLES**

> C2.12 M97020

CTH Y - CURVE 3

CTH Y - CURVE 1

STATION	LEFT	RIGHT	COMMENT
11+97.43	2.00% <	2.00% ->	END NORMAL CROWN - BEGIN SUPER
12+00	1.89% <	2.00%>	
12+45.43	0.00% -	2.00% ->	
12+50	0.19%>	2.00%>	
12+93.43	2.00% ->	2.00% ->	RC
13+00	2.27%>	2.27%>	
13+41.43	4.00%>	4.00%>	P.C. STATION
13+50	4.36% ->	4.36% ->	
13+89.43	6.00%>	6.00%>	BEGIN FULL SUPER
14+00	6.00% ->	6.00% ->	
14+50	6.00%>	6.00%>	
15+00	6.00% ->	6.00% ->	
15+50	6.00% ->	6.00% ->	
16+00	6.00%>	6.00%>	
16+50	6.00% ->	6.00% ->	
17+00	6.00%>	6.00%>	
17+50	6.00% ->	6.00% ->	
17+98.90	6.00%>	6.00%>	END FULL SUPER
18+00	5.95% ->	5.95% ->	
18+46.90	4.00% ->	4.00% ->	P.T. STATION
18+50	3.87%>	3.87%>	
18+94.90	2.00% ->	2.00% ->	
19+00	1.79%>	1.79%>	
19+42.90	0.00% -	2.00% ->	RC
19+50	0.30% <	2.00%>	
19+90.90	2.00% <	2.00% ->	BEGIN NORMAL CROWN - END SUPER

STATION	LEFT	RIGHT	COMMENT
21+34.79	2.00% <	2.00% ->	END NORMAL CROWN - BEGIN SUPER
21+50	2.00% <	1.40%>	
21+85.79	2.00% <-	0.00% -	
22+00	2.00% <-	0.56% <	
22+36.79	2.00% <	2.00% <	RC
22+50	2.52% <	2.52% <-	
22+87.79	4.00% <	4.00% <	P.C. STATION
23+00	4.48% <-	4.48% <-	
23+38.79	6.00% <-	6.00% <-	BEGIN FULL SUPER
23+50	6.00% <	6.00% <	
24+00	6.00% <-	6.00% <-	
24+50	6.00% <	6.00% <	
25+00	6.00% <-	6.00% <-	
25+50	6.00% <-	6.00% <-	
26+00	6.00% <	6.00% <	
26+50	6.00% <-	6.00% <-	
27+00	6.00% <	6.00% <	
27+50	6.00% <-	6.00% <-	
28+00	6.00% <	6.00% <	
28+50	6.00% <	6.00% <	
28+78.82	6.00% <-	6.00% <-	END FULL SUPER
29+00	5.17% <	5.17% <	
29+29.82	4.00% <-	4.00% <	P.T. STATION
29+50	3.21% <	3.21% <	
29+80.82	2.00% <	2.00% <	RC
30+00	2.00% <-	1.25% <-	
30+31.82	2.00% <	0.00%	
30+50	2.00% <-	0.71% ->	
30+82.82	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

CTH Y - CURVE 2

STATION	LEFT	RIGHT	COMMENT
53+39.72	2.00% <	2.00%>	END NORMAL CROWN - BEGIN SUPER
53+50	1.60% <	2.00% ->	
53+90.72	0.00%	2.00%>	
54+00	0.36% ->	2.00% ->	
54+41.80	2.00%>	2.00%>	RC
54+50	2.32% ->	2.32% ->	
54+86.06	3.73%>	3.73%>	P.C. STATION
55+00	4.28% ->	4.28% ->	
55+33.72	5.60%>	5.60%>	BEGIN FULL SUPER
55+50	5.60% ->	5.60% ->	
56+00	5.60%>	5.60%>	
56+50	5.60% ->	5.60% ->	
57+00	5.60%>	5.60%>	
57+50	5.60% ->	5.60% ->	
58+00	5.60%>	5.60%>	
58+50	5.60% ->	5.60% ->	
59+00	5.60%>	5.60%>	
59+50	5.60% ->	5.60% ->	
60+00	5.60%>	5.60%>	
60+50	5.60% ->	5.60% ->	
60+62.12	5.60%>	5.60%>	END FULL SUPER
61+00	4.12% ->	4.12% ->	
61+09.78	3.73%>	3.73%>	P.T. STATION
61+50	2.16% ->	2.16% ->	
61+54.04	2.00%>	2.00%>	RC
62+00	0.20% ->	2.00% ->	
62+05.12	0.00%	2.00%>	
62+50	1.76% <-	2.00% ->	
62+56.12	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

1	STATION	LEFT	RIGHT	COMMENT
	79+03.88	2.00% <-	2.00% ->	END NORMAL CROWN - BEGIN SUPER
	79+50	0.19% <	2.00%>	
	79+54.88	0.00% -	2.00% ->	
	80+00	1.77%>	2.00%>	
_	80+05.88	2.00% ->	2.00% ->	RC
_	80+22.88	2.67% ->	2.67% ->	P.C. STATION
	80+50	3.73%>	3.73%>	
	80+56.88	4.00% ->	4.00% ->	BEGIN FULL SUPER
_	81+00	4.00%>	4.00%>	
	81+50	4.00% ->	4.00% ->	
_	82+00	4.00%>	4.00%>	
_	82+50	4.00% ->	4.00% ->	
_	83+00	4.00% ->	4.00% ->	
_	83+50	4.00%>	4.00%>	
_	84+00	4.00% ->	4.00% ->	
_	84+50	4.00%>	4.00%>	
_	85+00	4.00% ->	4.00% ->	
_	85+50	4.00%>	4.00%>	
_	86+00	4.00% ->	4.00% ->	
_	86+18.18	4.00%>	4.00%>	END FULL SUPER
_	86+50	2.75%>	2.75%>	
_	86+52.18	2.67% ->	2.67% ->	P.T. STATION
_	86+69.18	2.00%>	2.00%>	RC
_	87+00	0.79% ->	2.00% ->	
_	87+20.18	0.00%	2.00%>	
_	87+50	1.17% <-	2.00% ->	
	87+71.18	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

CTH Y - CURVE 4

CTHY-CURVE 5

STATION	LEFT	RIGHT	COMMENT
90+82.52	2.00% <	2.00%>	END NORMAL CROWN - BEGIN SUPER
91+00	1.31% <	2.00% ->	
91+33.52	0.00%	2.00%>	
91+50	0.65%>	2.00%>	
91+84.43	2.00% ->	2.00% ->	RC
92+00	2.61%>	2.61%>	
92+26.86	3.67%>	3.67%>	P.C. STATION
92+50	4.58% ->	4.58% ->	
92+73.52	5.50%>	5.50%>	BEGIN FULL SUPER
92+00	5.50%>	5.50%>	
92+50	5.50% ->	5.50% ->	
93+00	5.50%>	5.50%>	
93+50	5.50%>	5.50%>	
94+00	5.50% ->	5.50% ->	
94+50	5.50%>	5.50%>	
95+00	5.50%>	5.50%>	
95+50	5.50% ->	5.50% ->	
96+00	5.50%>	5.50%>	
96+50	5.50%>	5.50%>	
96+51.67	5.50% ->	5.50% ->	END FULL SUPER
96+98.33	3.67%>	3.67%>	P.T. STATION
97+00	3.60%>	3.60%>	
97+40.76	2.00% ->	2.00% ->	RC
97+50	1.64%>	2.00%>	
97+91.67	0.00%	2.00%>	
98+00	0.33% <	2.00% ->	
98+42.67	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

CTHY-CURVE 6

STATION	LEFT	RIGHT	COMMENT
113+64.54	2.00% <-	2.00% ->	END NORMAL CROWN - BEGIN SUPER
114+00	0.61% <	2.00% ->	
114+15.54	0.00%~	2.00%>	
114+50	1.34% ->	2.00% ->	
114+66.88	2.00% ->	2.00% ->	RC
114+66.88	2.00%>	2.00%>	P.C. STATION
114+92.54	3.00% ->	3.00% ->	BEGIN FULL SUPER
115+00	3.00% ->	3.00% ->	
115+50	3.00%>	3.00%>	
116+00	3.00% ->	3.00% ->	
116+50	3.00% ->	3.00% ->	
117+00	3.00%>	3.00%>	
117+37.46	3.00% ->	3.00% ->	END FULL SUPER
117+50	2.51%>	2.51%>	
117+63.12	2.00%>	2.00%>	P.T. STATION
117+63.12	2.00% ->	2.00% ->	RC
118+00	0.56%>	2.00%>	
118+14.46	0.00%	2.00%>	
118+50	1.39% <	2.00% ->	
118+65.46	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

CTHY+CURVE 7

STATION	LEFT	RIGHT	COMMENT
127+26.87	2.00% <	2.00% ->	END NORMAL CROWN - BEGIN SUPER
127+50	2.00% <	0.95%>	
127+70.87	2.00% <	0.00% -	
128+00	2.00% <	1.31% <	
128+15.20	2.00% <	2.00% <	RC
128+50	3.57% <	3.57% <	
128+59.54	4.00% <	4.00% <	P.C. STATION
129+00	5.83% <	5.83% <	
129+03.87	6.00% <	6.00% <	BEGIN FULL SUPER
129+50	6.00% <-	6.00% <	
130+00	6.00% <	6.00% <	
130+50	6.00% <	6.00% <	
131+00	6.00% <-	6.00% <	
131+50	6.00% <	6.00% <	
132+00	6.00% <-	6.00% <	
132+50	6.00% <	6.00% <	
133+00	6.00% <	6.00% <	
133+50	6.00% <-	6.00% <	
134+00	6.00% <	6.00% <	
134+50	6.00% <	6.00% <	
135+00	6.00% <	6.00% <	
135+32.55	6.00% <-	6.00% <	END FULL SUPER
135+50	5.21% <	5.21% <	
135+76.88	4.00% <	4.00% <	P.T. STATION
136+00	2.96% <-	2.96% <	
136+21.22	2.00% <	2.00% <	RC
136+50	2.00% <	0.70% <	
136+65.55	2.00% <	0.00%	
137+00	2.00% <-	1.57% ->	
137+09.55	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

STATION	LEFT	RIGHT	COMMENT
144+27.15	2.00% <	2.00% ->	END NORMAL CROWN - BEGIN SUPER
144+50	1.10% <	2.00%>	
144+78.15	0.00% -	2.00% ->	
145+00	0.86%>	2.00%>	
145+29.22	2.00%>	2.00% <	RC
145+50	2.81% ->	2.81% ->	
145+73.48	3.73%>	3.73%>	P.C. STATION
146+00	4.77% ->	4.77% ->	
146+21.15	5.60%>	5.60%>	BEGIN FULL SUPER
146+50	5.60%>	5.60%>	
147+00	5.60% ->	5.60% ->	
147+50	5.60%>	5.60%>	
148+00	5.60% ->	5.60% ->	
148+50	5.60%>	5.60%>	
149+00	5.60% ->	5.60% ->	
149+50	5.60% ->	5.60% ->	
150+00	5.60%>	5.60%>	
150+50	5.60% ->	5.60% ->	
151+00	5.60%>	5.60%>	
151+50	5.60% ->	5.60% ->	
152+00	5.60% ->	5.60% ->	
152+50	5.60%>	5.60%>	
152+71.51	5.60% ->	5.60% ->	END FULL SUPER
153+00	4.48%>	4.48%>	
153+19.18	3.73% ->	3.73% ->	P.T. STATION
153+50	2.53%>	2.53%>	
153+63.44	2.00%>	2.00%>	RC
154+00	0.57% ->	2.00% ->	
154+14.51	0.00%	2.00%>	
154+50	1.39% <	2.00% ->	
154+65.51	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

CTH Y - CURVE 8

CTHY-CURVE 9

	STATION	LEFT	RIGHT	COMMENT
	167+07.78	2.00% <	2.00% ->	END NORMAL CROWN - BEGIN SUPER
	167+50	0.34% <	2.00%>	
	167+58.78	0.00%-	2.00% ->	
	168+00	1.62% ->	2.00% ->	
ı	168+09.78	2.00%>	2.00% <	RC
ı	168+50	3.58% ->	3.58% ->	
ı	168+60.78	4.00%>	4.00%>	P.C. STATION
ı	169+00	5.54% ->	5.54% ->	
1	169+11.78	6.00%>	6.00%>	BEGIN FULL SUPER
ı	169+50	6.00%>	6.00%>	
	170+00	6.00% ->	6.00% ->	
ı	170+50	6.00%>	6.00%>	
-	171+00	6.00% ->	6.00% ->	
	171+50	6.00%>	6.00%>	
ı	172+00	6.00%>	6.00%>	
Ì	172+50	6.00% ->	6.00% ->	
1	173+00	6.00%>	6.00%>	
	173+50	6.00% ->	6.00% ->	
Ì	174+00	6.00%>	6.00%>	
ı	174+50	6.00% ->	6.00% ->	
ı	175+00	6.00% ->	6.00% ->	
ı	175+50	6.00%>	6.00%>	
Ì	175+86.00	6.00% ->	6.00% ->	END FULL SUPER
ı	176+00	5.45%>	5.45%>	
1	176+37.00	4.00% ->	4.00% ->	P.T. STATION
ı	176+50	3.49% ->	3.49% ->	
Ì	176+88.00	2.00%>	2.00%>	RC
Ì	177+00	1.53% ->	2.00% ->	
	177+39.00	0.00%	2.00%>	
ı	177+50	0.43% <	2.00% ->	
ı	177+90.00	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

CTH Y - CURVE 10

STATION	LEFT	RIGHT	COMMENT
178+32.73	2.00% <	2.00%>	END NORMAL CROWN - BEGIN SUPER
178+50	2.00% <	1.32% ->	
178+83.73	2.00% <	0.00% ~	
179+00	2.00% <	0.64% <	
179+34.64	2.00% <	2.00% <	RC
179+50	2.60% <	2.60% <	
179+77.06	3.67% <	3.67% <	P.C. STATION
180+00.00	4.57% <	4.57% <	
180+23.73	5.50% <	5.50% <	BEGIN FULL SUPER
180+50	5.50% <	5.50% <	
181+00	5.50% <	5.50% <	
181+50	5.50% <	5.50% <	
182+00	5.50% <	5.50% <	
182+50	5.50% <	5.50% <	
183+00	5.50% <	5.50% <	
183+50	5.50% <	5.50% <	
183+50.84	5.50% <	5.50% <	END FULL SUPER
183+97.51	3.67% <	3.67% <	P.T. STATION
184+00	3.57% <	3.57% <	
184+39.93	2.00% <	2.00% <	RC
184+50	2.00% <	1.60% <	
184+90.84	2.00% <	0.00%	
185+00	2.00% <	0.36%>	
185+41.84	2.00% <	2.00% ->	BEGIN NORMAL CROWN - END SUPER

CTH Y - CURVE 11

	STATION	LEFT	RIGHT	COMMENT
	191+32.71	2.00% <	2.00% ->	END NORMAL CROWN - BEGIN SUPER
	191+50	2.00% <	1.32%>	
	191+83.71	2.00% <	0.00%	
	192+00	2.00% <-	0.64% <	
	192+34.62	2.00% <	2.00% <	RC
	192+50	2.60% <	2.60% <	
	192+77.04	3.67% <	3.67% <	P.C. STATION
	193+00	4.57% <	4.57% <	
	193+23.71	5.50% <	5.50% <	BEGIN FULL SUPER
	193+50	5.50% <	5.50% <	
	194+00	5.50% <-	5.50% <	
	194+50	5.50% <	5.50% <	
	195+00	5.50% <	5.50% <	
	195+50	5.50% <-	5.50% <	
	196+00	5.50% <	5.50% <	
	196+50	5.50% <	5.50% <	
	197+00	5.50% <	5.50% <	
	197+50	5.50% <	5.50% <	
ı	197+67.23	5.50% <	5.50% <	END FULL SUPER
	198+00	4.21% <	4.21% <	
	198+13.89	3.67% <-	3.67% <	P.T. STATION
	198+50	2.25% <	2.25% <	
ı	198-56.32	2.00% <	2.00% <	RC
	199+00	2.00% <-	0.28% <	
	199+07.23	2.00% <	0.00%	
	199+50	2.00% <	1.68% ->	
	199+58.23	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

CTH Y - CURVE 12

STA	TION	LEFT	RIGHT	COMMENT
199+	51.70	2.00% <	2.00% ->	END NORMAL CROWN - BEGIN SUPER
200)+00	2.00% <	0.11%>	•
200+	02.70	2.00% <	0.00% -	
200	+50	2.00% <	1.85%>	•
200+	53.70	2.00% <	2.00% <	RC
201	1+00	3.82% <	3.82% <-	•
201+	04.70	4.00% <	4.00% <	P.C. STATION
201	l+50	5.78% <	5.78% <-	
201+	55.70	6.00% <	6.00% <	BEGIN FULL SUPER
202	2+00	6.00% <	6.00% <-	•
202	2+50	6.00% <	6.00% <	•
203	3+00	6.00% <	6.00% <-	-
203	3+50	6.00% <	6.00% <	•
204	1+00	6.00% <	6.00% <	•
204	1+50	6.00% <	6.00% <-	•
205	5+00	6.00% <	6.00% <	
205	5+50	6.00% <	6.00% <	•
205+	60.28	6.00% <	6.00% <	END FULL SUPER
206	S+00	4.44% <	4.44% <-	•
206+	11.27	4.00% <	4.00% <	P.T. STATION
206	3+50	2.48% <	2.48% <	•
206+	62.28	2.00% <	2.00% <-	RC
207	7+00	2.00% <	0.52% <	
207+	13.28	2.00% <	0.00% -	
207	7+50	2.00% <	1.44%>	•
2074	6428	2.00% <-	2.00% ->	BEGIN NORMAL CROWN - END SUPER

CTH Y - CURVE 14

STATION	LEFT	RIGHT	COMMENT
211+57.58	2.00% <	2.00%>	END NORMAL CROWN - BEGIN SUPER
212+00	0.07% <	2.00%>	
212+01.58	0.00%	2.00%>	
212+45.91	2.00% ->	2.00% ->	RC
212+50	2.18%>	2.18%>	
212+90.24	4.00% ->	4.00%>	P.C. STATION
213+00	4.44%>	4.44%>	
213+34.58	6.00%>	6.00%>	BEGIN FULL SUPER
213+50	6.00% ->	6.00%>	
214+00	6.00%>	6.00%>	
214+50	6.00% ->	6.00% ->	
215+00	6.00%>	6.00%>	
215+50	6.00% ->	6.00%>	
216+00	6.00%>	6.00%>	
216+50	6.00% ->	6.00% ->	
217+00	6.00%>	6.00%>	
217+49.43	6.00% ->	6.00%>	END FULL SUPER
217+50	5.97%>	5.97%>	
217+93.77	4.00% ->	4.00% ->	P.T. STATION
218+00	3.72%>	3.72%>	
218+38.10	2.00% ->	2.00%>	RC
218+50	1.46%>	1.46%>	
218+82.43	0.00%-	2.00% ->	
219+00	0.80% <	2.00%>	
219+26.43	2.00% <	2.00%>	BEGIN NORMAL CROWN - END SUPER

STATION	LEFT	RIGHT	COMMENT
232+10.34	2.00% <-	2.00% ->	END NORMAL CROWN - BEGIN SUPER
232+50	2.00% <	0.44%>	
232+61.34	2.00% <	0.00% ~	
233+00	2.00% <-	1.51% <-	
233+12.57	2.00% <	2.00% <	RC
233+50	3.46% <	3.46% <	1
233+58.68	3.80% <	3.80% <	P.C. STATION
234+00	5.41% <	5.41% <	
234+07.34	5.70% <-	5.70% <-	BEGIN FULL SUPER
234+50	5.70% <	5.70% <	
235+00	5.70% <-	5.70% <-	
235+50	5.70% <	5.70% <	
236+00	5.70% <	5.70% <	
236+50	5.70% <-	5.70% <-	
237+00	5.70% <	5,70% <	
237+50	5.70% <-	5.70% <-	
238+00	5.70% <	5.70% <	
238+50	5.70% <	5.70% <	
239+00	5.70% <-	5.70% <-	
239+50	5.70% <	5.70% <	
240+00	5.70% <	5.70% <	
240+50	5.70% <	5.70% <	
241+00	5.70% <	5.70% <	
241+50	5.70% <	5.70% <-	
242+00	5.70% <	5.70% <	
242+50	5.70% <	5.70% <-	
243+00	5.70% <	5.70% <-	
243+50	5.70% <	5.70% <	
243+50.14	5.70% <	5.70% <-	END FULL SUPER
243+98.80	3.80% <	3.80% <	P.T. STATION
244+00	3.75% <	3.75% <	
244+44.91	2.00% <	2.00% <-	RC
244+50	2.00% <	1.80% <	
244+96.14	2.00% <	0.00%	
245+00	2.00% <	0.15%>	
245+47.14	2.00% <-	2.00% ->	BEGIN NORMAL CROWN - END SUPER

MARATHON COUNTY, WISCONSIN 153 -CTH STH

560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

2/28/2013

Drawing Name SUPERELEVATION TABLES

C2.13

CTH Y CTH Y

		CTHY	
C1-1:	Y		BENADYS
Station 10+00.00	133,341.5710°	X 343,728.1400'	REMARKS
10+50.00	133,391.54001		BEGIN CONSTRUCTION
11+00.00	133,441.5031		
12+00.00	133,541.4352		
13+00.00 13+41.43	133,641.3673	343,739.1936' 343,740.7200'	
14+00.00	133,682.7700		
15+00.00	133,840.1061		
15+99.69	133,940.85001	343,750.2400	PI STATION
16+00.00	133,937.0692	343,783.3613	
17+00.00	134,031.12331	343,817.2064	
18+00.00 18+46.90	134,121.3287' 134,162.0500'	343,860,2721' 343,883,5300'	
19+00.00	134,207.5330	343,910.9337	
20+00.00	134,293.1846	343,962.5457	
21+00.00	134,378.8362	344,014.1577	
22+00.00	134,464.4878	344,065.7698	
22+87.79	134,539.68001	344,111.0800	PC STATION
23+00.00	134,550.1748	344,117.3225' 344,163.9009'	
25+00.00	134,731.0015	344,202.1018	
26+00.00	134,826.5151	344,231.5977'	
26+18.61	134,823.03001	344,281.8200	
27+00.00	134,924.3468	344,252.1361'	
28+00.00	135,023.65831	344,263.5410	
29+00.00 29+29.82	135,123.5990° 135,153.4000°	344,265.7146' 344,264.5700'	PT STATION
30+00.00	135,223.4806	344,260.9119	OTOTION
31+00.00	135,323.3446	344,255.6977	
32+00.00	135,423.2086	344,250.4835	
33+00.00	135,523.0726	344,245.2693	
34+00.00	135,622.9365	344,240.0552	<u> </u>
35+00.00 36+00.00	135,722.8005	344,234.8410° 344,229.6268°	
37+00.00	135,922.5284	344,224,4126	
38+00.00	136,022.39241	344,219.1985	
39+00.00	136,122.26731	344,214.1994	
40+00.00	136,222.1470	344,209.2966'	
41+00.00	136,322.0267	344,204.3939	
42+00.00 43+00.00	136,421.9065'	344,199.4911' 344,194.5883'	
44+00.00	136,621.6660	344,189.6856	
45+00.00	136,721.5457	344,184.7828	
46+00.00	136,821.4255	344,179.8800'	
47+00.00	136,921.3052	344,174.9773	
48+00.00	137,021.18491	344,170.0745	
49+00.00 50+00.00	137,121.0647' 137,220.9444'	344,165.1717' 344,160.2689'	
51+00.00	137,320.8242	344,155.3662	
52+00.00	137,420.70391	344,150.4634	
53+00.00	137,520.5837	344,145.5606	
54+00.00	137,620.46341	344,140.6579	
54+86.06	137,706.4200		
55+00.00 56+00.00	137,720.3461	344,135.8190° 344,135.1211°	
57+00.00	137,920.1347		
58+00.00		344,153.4231	
58+02.37	138,022.35001	344,120.9300	PISTATION
59+00.00	138,117.51701	344,172.3438	
60+00.00	138,214,23601	344,197.6777'	
61+00.00 61+09.78	138,309.0804° 138,318.2400°	344,229.3154' 344,232.7400'	
62+00.00	138,402.6354	344,264.6346	
63+00.00	138,496.1794	344,299.9832	
64+00.00	138,589.7234	344,335.3318	
65+00.00	138,683.26741	344,370.6804	
66+00.00	138,776.81131	344,406.0290	
67+00.00 68+00.00	138,870.3553° 138,963.8993°	344,441.3776' 344,476.7262'	
69+00.00	139,057.4484	344,512.0613	
70+00.00	139,151.1942	344,546.8711	
71+00.00	139,244.9400	344,581.6810	
72+00.00	139,338.68581	344,616.4908	
73+00.00	139,432.4316° 139,526.1774°	344,651.3006' 344,686.1105'	
75+00.00	139,619.9232	344,720.9203	
76+00.00	139,713.6690	344,755.7301	
77+00.00	139,807.4148	344,790.5399	
78+00.00	139,901.1606	344,825.3498	
79+00.00 80+00.00	139,994.9064	344,860.1596	
80+22.88	140,088.6522	344,894.9694° 344,902.9300°	
81+00.00	140,182.0259	344,930.7542	
82+00.00	140,274.12161	344,969.7076	
83+00.00	140,364.7941	345,011.8677'	
83+38.81	140,406.2800	345,012.9100	PISTATION
85+00.00	140,453.9317	345,057.1827	
85+00.00 86+00.00	140,541.4248	345,105.5969° 345,157.0505°	
86+52.18	140,671.1800	345,185.0900	PT STATION

		CTH Y	
Station			DEMADE:
7+00.00	Y 140,711.2693'	X 345,211.1452'	REMARKS
8+00.00	140,795.1149	345,265.6420'	
9+00.00	140,878.9606	345,320.1388	
0+00.00+0	140,962.8063	345,374.6357'	
1+00.00	141,046.6520	345,429.1325'	
2+00.00	141,130.4976' 141,153.0100'	345,483.6293' 345,498.2600'	PC STATION
3+00.00	141,153,0100	345,539.5138'	FOSTATION
4+00.00	141,292.8053	345,600.2862'	
4+64.32	141,352,1100	345,627.6700'	PISTATION
5+00.00	141,368.2488	345,665.8989'	
6+00.00	141,439,4468	345,736.0957'	
6+98.33	141,505.0500'	345,809.33001	PT STATION
8+00.00	141,506.1221' 141,570.5264'	345,810.6018'	
9+00.00	141,634.9307	345,887.1007' 345,963.5996'	
00.00+00	141,699.3350'	346,040.0985'	
01+00.00	141,763.7393	346,116.5975'	
02+00.00	141,828.1436	346,193.0964'	
03+00.00	141,892.5479	346,269.5953'	
04+00.00	141,956.9522'	346,346.0943'	
05+00.00 06+00.00	142,021.3565' 142,085.7608'	346,422.5932' 346,499.0921'	
7+00.00	142,150.1651	346,575.5910	
00.00+80	142,214.5694	346,652.0900'	
09+00.00	142,278.9737	346,728.5889'	
10+00.00	142,343.3780	346,805.0878'	
11+00.00	142,407.7823	346,881.5868'	
12+00.00	142,472.1866	346,958.0857	
13+00.00	142,536.5909'	347,034.5846'	
14+00.00 14+68.88	142,600.9952' 142,644.0700'	347,111.0835' 347,162.2400'	PCSTATION
15+00.00	142,665.3012	347,187.6648'	FOURTOR
16+00.00	142,728.2045	347,265.3997'	
16+15.06	142,739.5000'	347,275.6000'	PI STATION
17+00.00	142,789.2725	347,344.5846'	
17+63.12	142,826.8600'	347,395.29001	PT STATION
18+00.00 19+00.00	142,848.6004'	347,425.0828'	
20+00.00	142,907.5533' 142,966.5062'	347,505.8575' 347,586.6322'	
21+00.00	143,025.4591	347,667.4069'	
22+00.00	143,084.4120	347,748.1817'	
23+00.00	143,143.3649	347,828.9564'	
24+00.00	143,202.3178	347,909.7311'	
25+00.00	143,261.2708	347,990.5058	
26+00.00	143,320,2237	348,071.2805'	
27+00.00 28+00.00	143,379.1766' 143,438.1295'	348,152.0552' 348,232.8300'	
28+59.53	143,473.2300	348,280.9200	POSTATION
29+00.00	143,497.9524	348,312.9454'	
30+00.00	143,566.1459	348,385.9854'	
31+00.00	143,643.4439	348,449.3116'	
32+00.00	143,728.4744'	348,501.7998'	DISTATION
32+48.31	143,702,4200	348,594.9500'	PISTATION
33+00.00 34+00.00	143,819.7279' 143,915.5845'	348,542.5183' 348,570.7442'	
35+00.00	144,014.3426	348,585.9766	
35+76.88	144,091.1500	348,588.6700'	PT STATION
36+00.00	144,114.2586	348,588.3012'	
37+00.00	144,214.2456'	348,586.68671	
00.00+8	144,314.2325	348,585.0722'	
9+00.00	144,414.2195	348,583.4577'	
0+00.00	144,514.2064'	348,581.8432'	
11+00.00 12+00.00	144,614.1934' 144,714.1804'	348,580.2288'	
13+00.00	144,814.1673	348,578.6143' 348,576.9998'	
4+00.00	144,914,1543	348,575.3853'	
5+00.00	145,014.1413	348,573.7708	
5+73.48	145,087.6200'	348,572.5800'	PC STATION
6+00.00	145,114.1307	348,572.3907'	
7+00.00	145,214.0514	348,575.8759'	
00.00+8	145,313.5180'	348,586.0097'	
19+00.00 19+54.20	145,412.0885'	348,602.7473' 348,566.4400'	PISTATION
0+00.00	145,468.2900' 145,509.3251'	348,626.0142'	PISTATION
1+00.00	145,604.7956	348,655.7070	
52+00.00	145,698.0761	348,691.6939'	
3+00.00	145,788.7520	348,733.8149'	
53+19.18	145,805.8100	348,742.5800'	PT STATION
54+00.00	145,877.4623	348,779.9710'	
55+00.00	145,966.1164	348,826.2360	
56+00.00	146,054.7705'	348,872.5011'	
57+00.00 58+00.00	146,143.4246' 146,232.0786'	348,918.7661' 348,965.0312'	
59+00.00	146,320.7327	349,011.2962'	
00.00+06	146,409.3868	349,057.5613'	
30.00,001			
31+00.00	146,498.0409'	349,103.8263'	
	146,498.0409 146,586.6950 146,675.3490	349,150.0913' 349,196.3564'	

Station	Y X	REMARKS
165+00.00	146,852.6572' 349,288.8865'	
166+00.00	146,941.3113' 349,335.1515'	
167+00.00	147,029.9654' 349,381.4165'	
168+00.00	147,118.6194' 349,427.6816'	DO OTATION
168+60.78 169+00.00	147,172.5000' 349,455.8000' 147,206.9643' 349,474.5235'	PC STATION
170+00.00	147,291.8277' 349,527.3645'	
171+00.00	147,371.8922' 349,587.2279'	
172+00.00	147,446.5784' 349,653.6804'	
172+63.65	147,529.6600' 349,642.1900'	PI STATION
173+00.00	147,515.3456' 349,726.2410'	
174+00.00	147,577.6959' 349,804.3845'	
175+00.00	147,633.1781' 349,887.5451'	
176+00.00	147,681.3905' 349,975.1210'	
176+37.00	147,697.3200' 350,008.5200'	PT STATION
177+00.00 178+00.00	147,723.5320' 350,065.8029' 147,765.1463' 350,156.7328'	
179+00.00	147,806.7605' 350,247.6628'	
179+77.06	147,838.8300' 350,317.7300'	PCSTATION
180+00.00	147,848.5288' 350,338.5213'	
181+00.00	147,894.3665' 350,427.3776'	
181+88.58	147,926.8500' 350,510.0700'	PI STATION
182+00.00	147,945.8375' 350,513.0938'	
183+00.00	148,002.7276' 350,595.3133'	
183+97.50	148,063.1900' 350,671.7900'	PT STATION
184+00.00	148,064,7987' 350,673,6952'	
185+00.00 186+00.00	148,129.2552', 350,750.1502', 148,193.7117', 350,826.6051'	
187+00.00	148,258.1682' 350,903.0601'	
188+00.00	148,322.6247' 350,979.5151'	
189+00.00	148,387.0811' 351,055.9700'	
190+00.00	148,451.5376' 351,132.4250'	
191+00.00	148,516.0995' 351,208.7909'	
192+00.00	148,580.7027' 351,285.1220'	PC STATION
192+77.04	148,630.4800' 351,343.9300'	
193+00.00	148,645.4311' 351,361.3460'	
194+00.00	148,713.4355', 351,434.6410'	DISTATION
195+00.00 195+48.02	148,785.8850' 351,503.5453' 148,805.5300' 351,550.7700'	PI STATION
196+00.00	148,862.4968' 351,567.7900'	
197+00.00	148,942.9716' 351,627.1242'	
198+00.00	149,026.9954' 351,681.3160'	PT STATION
198+13.89	149,038.9300' 351,688.4300'	
199+00.00	149,113.0988' 351,732.1704'	
200+00.00	149,199.2330' 351,782.9729'	
201+00.00	149,285.3673' 351,833.7754'	PCSTATION
201+04.70	149,289.4200' 351,836.1700'	
202+00.00	149,373.4946' 351,880.9640' 149,465.5378' 351,919.9663'	PI STATION
203+62.56	149,511.5200' 351,967.1600'	TICIATION
204+00.00	149,560.7416' 351,950.4516'	
205+00.00	149,658.3198' 351,972.1680'	
206+00.00	149,757.4666' 351,984.9361'	PT STATION
206+11.27	149,768.7100' 351,985.8100'	
207+00.00	149,857.2005; 351,992.2246;	
208+00.00	149,956.9387' 351,999.4554'	
210+00.00	150,056.6769' 352,006.6863' 150,156.4152' 352,013.9171'	
210+00.00 211+00.00	150,156.4152' 352,013.9171' 150,256.1534' 352,021.1479'	
212+00.00	150,355.8916' 352,028.3788'	PC STATION
212+90.24	150,445.9000' 352,034.9000'	, a c.milait
213+00.00	150,455.6253' 352,035.6689'	
214+00.00	150,554.4814' 352,050.3132'	
215+00.00	150,650.7405' 352,077.1680'	PI STATION
215+50.66	150,705.6300' 352,053.7300'	
216+00.00	150,742.9004' 352,115.8143'	
217+00.00	150,829.5230' 352,165.6492'	PT STATION
217+93.77	150,904.5100' 352,221.8500'	
218+00.00 219+00.00	150,909.2723' 352,225.8763' 150,985.6418' 352,290.4341'	
220+00.00	151,062.0113' 352,354.9918'	
221+00.00	151,138.3808' 352,419.5495'	
222+00.00	151,214.7503' 352,484.1072'	
223+00.00	151,291.1198' 352,548.6649'	
224+00.00	151,367.4893' 352,613.2226'	
225+00.00	151,443.8588' 352,677.7803'	
226+00.00	151,520.2283' 352,742.3381'	
227+00.00	151,596.5978' 352,806.8958'	
228+00.00	151,673.0083' 352,871.4050'	
229+00.00	151,749.4390' 352,935.8903' 151,825.8697' 353,000.3755'	
230+00.00 231+00.00	151,825.8697' 353,000.3755' 151,902.3004' 353,064.8608'	
232+00.00	151,978.7311' 353,129.3461'	
	152,055.1618' 353,193.8313'	POSTATION
233+00.00	152,100.0100' 353,231.6700'	
233+00.00 233+58.68		
	152,131.9653' 353,257.8661'	
233+58.68 234+00.00 235+00.00	152,131.9653' 353,257.8661' 152,212.2618' 353,317.4359'	
233+58.68 234+00.00	152,131.9653' 353,257.8661'	

Station	Y	x	REMARKS
	152,568.3065		PISTATION
	152,515.2100*	353,581.98001	
	152,663.9443' 152,761.3526'		
	152,860.0747' 152,959.6475'	353,564.4655 ¹ 353,573.4849 ¹	DT STATION
	153,058.4100	353,575.6800	PT STATION
	153,059.6041		
	153,059.5974		
	153,259.5906		
	153,359.5839		
	153,459.5772	353,571.0352	
	153,559.5705	353,569.8767	
	153,659.5638'		
	153,759.5571"		
	153,859.5504		
	153,959.5437		
	154,059.5370		
	154,159.5303		
	154,259.5235"		
257+00.00	154,359.5168	353,560.60911	
	154,459.5105'		
	154,559.5052		
260+00.00	154,659.5000	353,557.4330	
261+00.00	154,759.4947	353,556.40891	
	154,859.4895		
	154,959.4842'		
	155,059.4790'		
	155,159.4738		
	155,259.4685		
	155,359.4633'		
	155,459.4580'	,	
	155,559.4528		
	155,659.4475		
	155,759.4423		
	155,859.4370		
	155,959.4318' 156,059.4266'		
	156,159.4213		
	156,259.4161		
	156,359.4108		
	156,459.4056		
	156,559.4003		
	156,659.3951		
	156,759.3898		
	156,859.3846'		
	156,959.3755"		
	157,059.3656		
	157,159.3556'		
	157,259.3457		
	157,359.3358		
	157,459.3259		
289+00.00	157,559.3160		
290+00.00	157,659.3061		
	157,759.2962		
	157,859.2862		
	157,959.2763		
294+00.00	158,059.2664	353,518.0706	
	158,159.2565'		
	158,259.2466'		
	158,359.2367		
	158,459.2268		
	158,559,2169		
	158,659.2069		
	158,759.1970		
	158,859,1871		
	158,959.1772		
	159,059,1673		
	159,159.1574' 159,259.1475'		
200.00.00	159,359.1473	353 499 9910	
40 / + DH DO			



560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

THIS DOCUMENT, THE IDEAS AND DESIGNS INCORPORA HEREIN AS AN INSTRUMENT OF SERVICE, IS THE PROPH OF JEWELL ASSOCIATES ENGINEERS, INC. AND IS NOT T BE USED IN WHOLE OR IN PART WITHOUT PRIOR WRITT AUTHORIZATION OF INSULAL ASSOCIATES ENCINEERS.

CTH Y STH 153 - CTH II MARATHON COUNTY, WISCONSIN

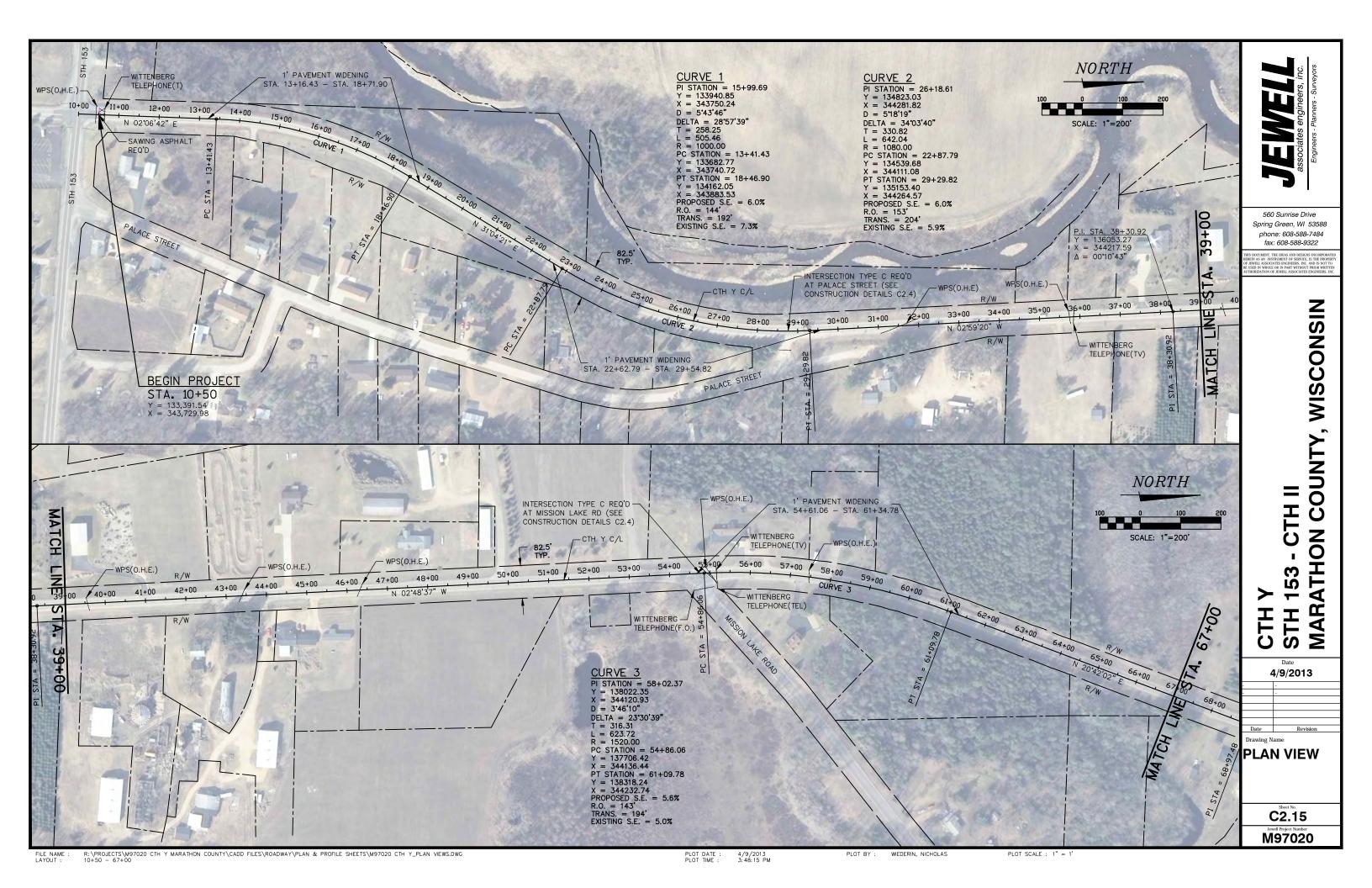
4/17/2013

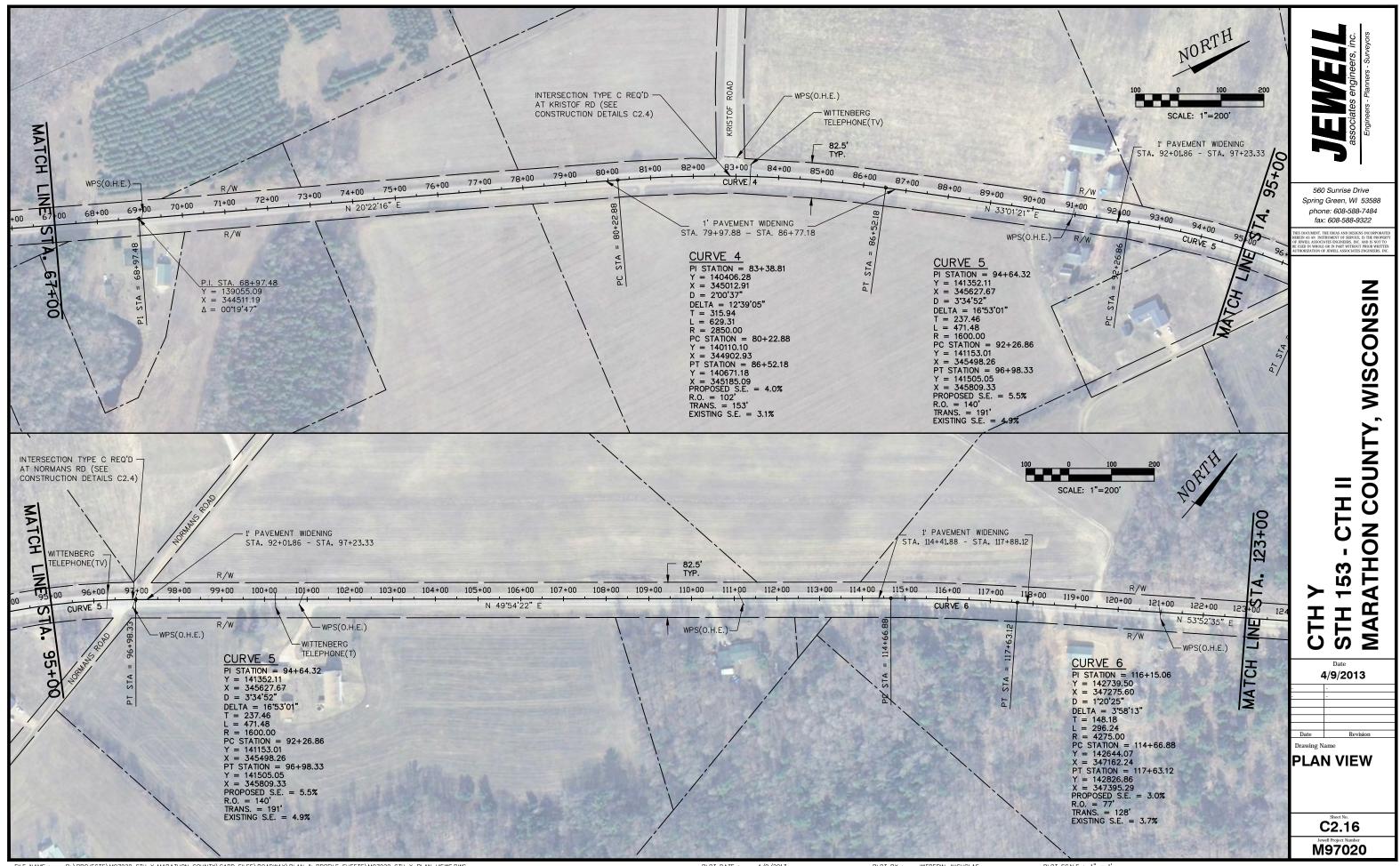
-

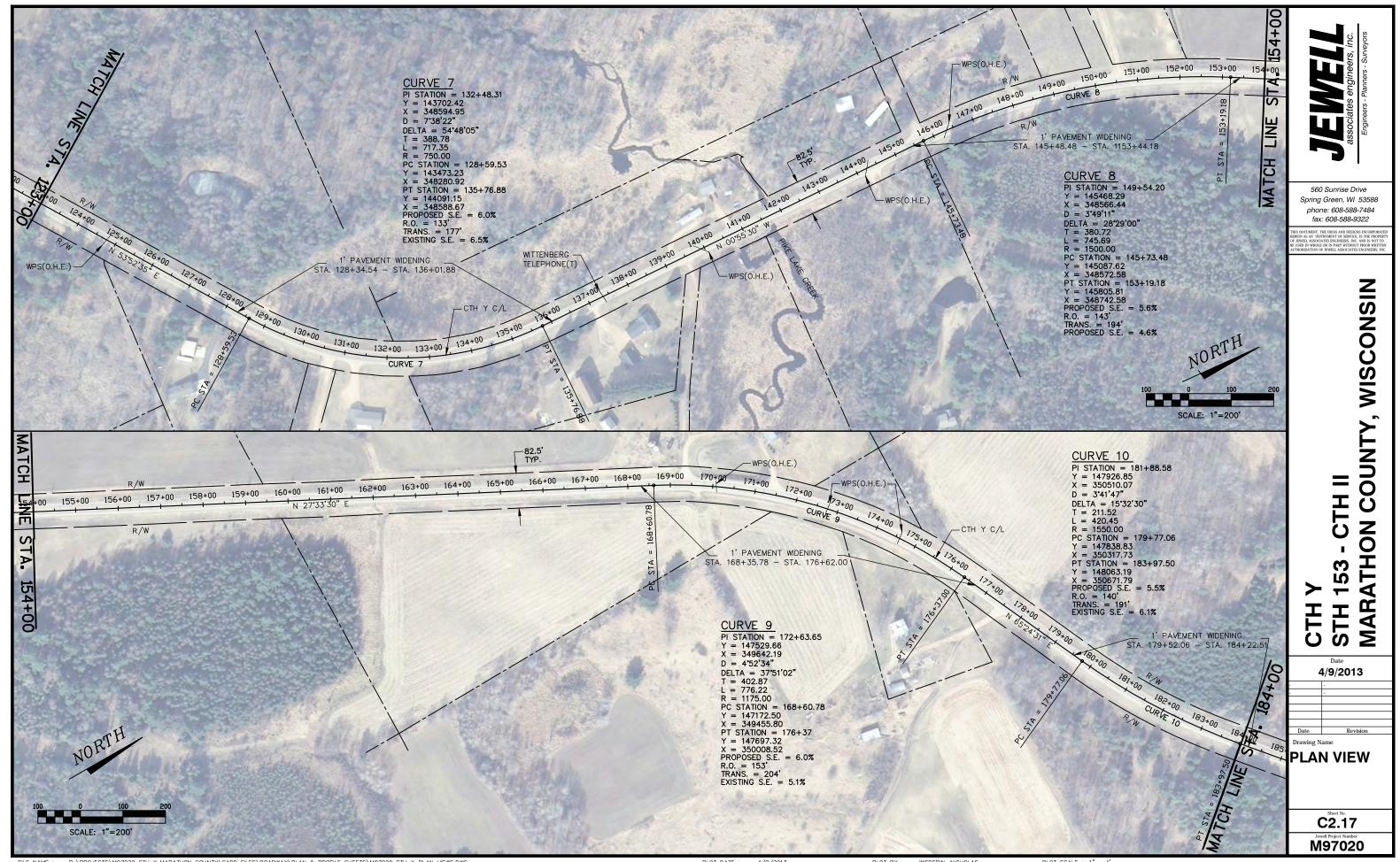
Date
Drawing Name

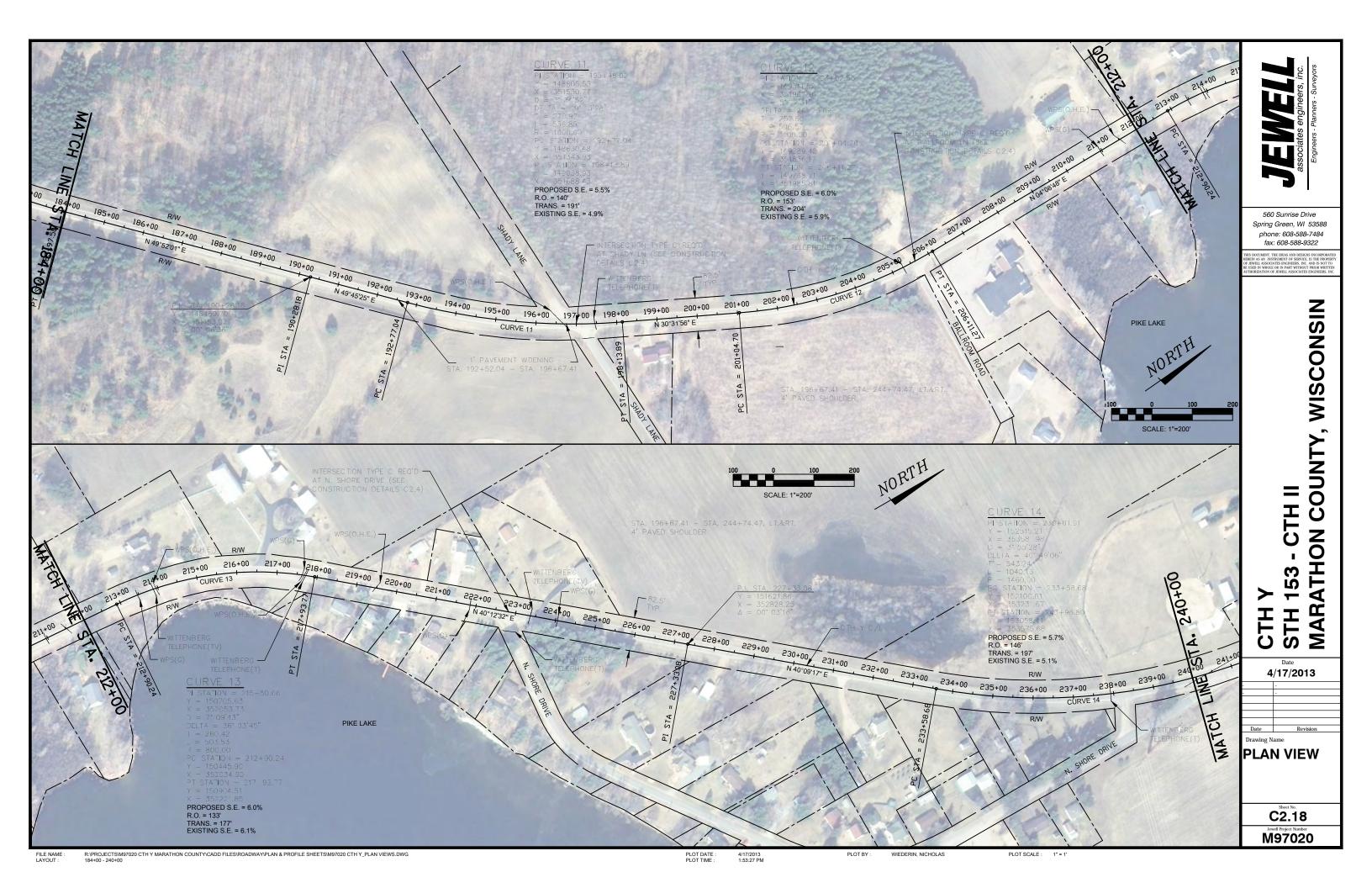
Station/Coordinate Table

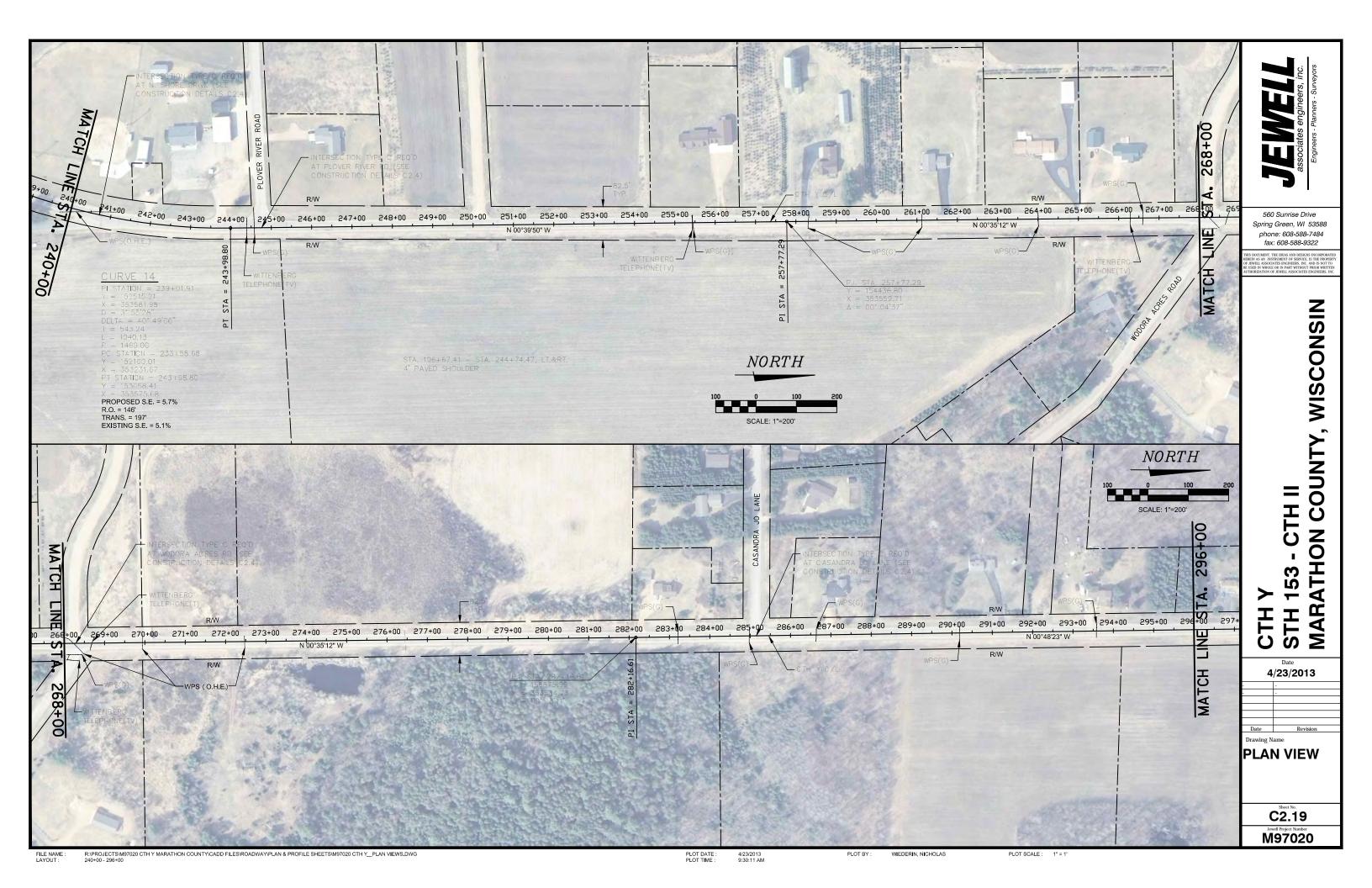
C2.14













FILE NAME : LAYOUT :

4/17/2013 1:55:02 PM

REMOVING ASPHALTIC SURFACE BUTT JOINTS

		204.0115
STATION-STATION	LOCATION	(SY)
10+50 - 11+25	CTHY	200
307+15 - 307+90	CTHY	200
_	PALACE STREET	154
-	MISSION LAKE RD	342
-	KRISTOF RD	296
-	NORMANS RD (SOUTH)	93
-	SHAOY LN (EAST)	135
_	SHADY LN (WEST)	201
-	BALLROOM LN	148
-	N. SHORE DR (222+71)	186
-	N. SHORE DR (238+22)	124
_	N. SHORE DR (240+38)	296
	PLOVER RIVER RO	251
-	WODORA ACRES RD	146
-	CASANDRAM JO LN	135
_	СТН II	378
-	BRIDGE RD	144
-	P.E., F.E., & C.E. (27)	1871
	TOTAL =	5300

REMOVING ASPHALTIC SURFACE MILLING

		204.0120
STATION - STATION	LOCATION	(SY)
11+25 - 307+15	CTHY	79000
	TOTAL =	79000

FINISHING ROADWAY

	213.0100
LOCATION	(EACH)
PROJECT M97020	1
TOTAL =	1

BASE AGGREGATE DENSE

		305.0110 3/4-INCH	305.0120 1 1/4-INCH
STATION - STATION	LOCATION	(TON)	(TON)
10+50 - 307+90	CTH Y, LT & RT	4586	-
11+97.43 - 19+90.90	CTH Y, CURVE 1	-	308
21+34.79 - 30+82.82	CTH Y, CURVE 2	-	47
53+39.72 - 62+56.12	CTH Y, CURVE 3	-	172
79+03.88 - 87+71.18	CTH Y, CURVE 4	-	285
90+82.52 - 98+42.67	CTH Y, CURVE 5	_	130
113+64.54 - 118+65.46	CTH Y, CURVE 6	-	87
127+26.87 - 137+09.55	CTH Y, CURVE 7	-	174
144+27.15 - 154+65.51	CTH Y, CURVE 8	_	335
167+07.78 - 177+90.00	CTH Y, CURVE 9	-	316
178+32.73 - 185+41.84	CTH Y, GURVE 10	-	114
191+32.71 - 199+58.23	CTH Y, CURVE 11	-	148
199+51.70 - 207+64.28	CTH Y, CURVE 12	-	37
211+57.58 - 219+26.43	CTH Y, GURVE 13	-	40
232+10.34 - 245+47.14	CTH Y, CURVE 14	-	282
_	NORMANS RD (NORTH)	12	_
	P.E., F.E., C.E. (68)	526	-
	UNDISTRIBUTED	176	75
	TOTALS =	5300	2550

PULVERIZE AND RELAY

STATION - STATION 11+25 - 307+15	LOCATION	325.0100 (\$Y) 79000
71 20 007 70	TOTAL=	79000

ASPHALTIC ITEMS

		455.0105 ASPHALTIC MATERIAL PG 58-28	455.0605 TACK COAT	460.1101 HMA PAVEMENT TYPE E-1
STATION - STATION	LOCATION	(TON)	(GAL)	(TON)
10+50 - 307+62	CTHY	922	2082	16755
_	PALACE STREET	2	4	31
_	MISSION LAKE RD	4	9	69
-	KRISTOF RD	3	7	60
-	NORMANS RD (SOUTH)	1	2	19
_	SHADY LN (EAST)	1	3	27
	SHADY LN (WEST)	2	5	40
_	BALLROOM LN	2	4	30
_	N. SHORE DR (222+71)	2	5	37
	N. SHORE DR (238+22)	1	3	2 5
<u></u>	N. SHORE DR (240+38)	3	7	60
_	PLOVER RIVER RD	3	6	50
-	WODORA ACRES RD	2	4	29
_	CASANDRAM JO LN	1	3	27
-	COUNTY RD II	4	9	76
_	BRIDGE RD	2	4	29
-	P.E., F.E., & C.E. (27)	17	-	300
-	UNDISTRIBUTED	28	43	536
	TOTAL =	1000	2200	18200

NOTE: ASPHALTIC CONTENT IS 5.5%
HMA PAVEMENT 115 LBS/(SQUARE YARD * INCH OF THICKNESS)
TACK COAT 0.025 GAL/SY

MOBILIZATION

LOCATION	619.1000 (EACH)
PROJECT M97020	1
TOTAL =	1

WATER

STATION - STATION	LOCATION	624.0100 (MGAL)
10+50 - 307+90	CTHY, LT & RT	106
-	P.E., F.E., C.E. (68)	8
-	UNDISTRIBUTED	6
	TOTALS =	120

TRAFFIC CONTROL

	643.0100
LOCATION	(EACH)
PROJECT M97020	1
TOTAL =	1

PAVEMENT MARKING

			646.0103 PAVEMENT MARKING PAINT 4-INCH	648.0100 LOCATING NO-PASSING ZONES	649.0100 TEMPORARY PAVEMENT MARKING 4-INCH
STATION - STATION	LOCATION	DESCRIPTION	(LF)	(MI)	(LF)
10+50 - 307+90	CTHY	-	-	5.63	-
10+50 - 307+90	СТН Ү, СЛ	YELLOW SKIP	-	-	2380
10+50 - 307+90	CTHY, RT	WHITE EDGELINE	28860	-	-
10+50 - 307+90	CTHY, LT	WHITE EDGELINE	29340	-	-
10+50 - 27+75	CTH Y, C/L	OOUBLE YELLOW	3450	-	-
27+75 - 40+50	CTH Y, C/L	NB PASSING ONLY	1590	-	-
40+50 - 46+50	CTH Y, C/L	PASSING	150	_	-
46+50 - 55+50	CTH Y, C/L	SB PASSING ONLY	1125	_	_
55+50 - 58+60	CTH Y, C/L	DOUBLE YELLOW	620	-	-
58+60 - 69+30	CTH Y, C/L	NB PASSING ONLY	1335	_	-
69+30 - 74+60	CTH Y, C/L	PASSING	130	-	-
74+60 - 83+25	CTH Y, C/L	SB PASSING ONLY	1080	_	_
83+25 - 96+10	СТН Ү, СЛ	OOUBLE YELLOW	2570		
96+10 - 104+45	CTH Y, C/L	NB PASSING ONLY	1040	_	_
104+45 - 112+60	CTH Y, C/L	PASSING	200	-	-
112+60 - 124+00	CTH Y, C/L	SB PASSING ONLY	1425	_	_
124+00 - 242+00	CTH Y, C/L	DOUBLE YELLOW	23600	_	_
242+00 - 252+25	CTH Y, C/L	NB PASSING ONLY	1280		-
252+25 - 280+00	CTH Y, C/L	PASSING	695	_	_
280+00 - 289+60	CTH Y, C/L	SB PASSING ONLY	1200	-	-
289+60 - 300+60	CTH Y, C/L	DOUBLE YELLOW	2200	_	_
300+60 - 307+90	CTH Y, C/L	NB PASSING ONLY	910	-	-
-	CTH II, C/L	DOUBLE YELLOW	100	_	-
_	CTH II, LT & RT	WHITE EDGELINE	100		
		TOTALS =	103000	5.63	2380

CONSTRUCTION STAKING

CTATION CTATION	LOCATION	650.8000 RESURFACING REFERENCE	650.9910 SUPPLEMENTAL CONTROL
10+50 - 307+90	CTHY	(LF) 29740	(LS) 1
	TOTALS =	29740	1

SAWING ASPHALT

		690.0150
STATION	LOCATION	(LF)
10+50	CTH Y	24
30790	CTHY	24
_	PALACE STREET	24
-	MISSION LAKE RD	22
-	KRISTOF RD	22
_	NORMANS RD (SOUTH)	20
-	SHADY LN (EAST)	22
_	SHADY LN (WEST)	22
_	BALLROOMLN	20
-	N. SHORE DR (222+71)	20
_	N. SHORE DR (238+22)	20
_	N. SHORE DR (240+38)	20
-	PLOVER RIVER RD	22
_	WODORA ACRES RD	22
-	CASANDRAM JO LN	22
_	CTH II	22
_	BRIDGE RD	22
-	P.E., F.E., & C.E. (27)	490
	TOTALS =	860

560 Sunrise Drive Spring Green, WI 53588 phone: 608-588-7484 fax: 608-588-9322

MARATHON COUNTY, WISCONSIN CTH

4/11/2013 Drawing Name MISCELLANEOUS QUANTITIES

M97020

C3.0